



Aberdeen Harbour Expansion Project

Construction Environmental Management Document

11th May 2017

DRAGADOS

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Chapter 6

Construction Traffic Management Plan

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6.1 Introduction

6.1.1 Construction Traffic Management Plan

The purpose of this document is to establish a set of procedures capable of meeting contractual and statutory requirements with respect to the management of construction traffic. These procedures are presented in the form of a Construction Traffic Management Plan (CTMP) and are designed to reduce the potential impact of construction activities on the surrounding traffic and transportation networks.

6.1.2 Roles, Responsibilities and Cross-Referencing

Table 6.1 details the responsibility of selected project staff with regards to construction traffic management.

Table 6.1 Roles and Responsibility Table

Job Title	Name	Responsibilities
Traffic Safety and Control Officer	Anthony Gannon	Manage local permits/consents with Aberdeen City Council (ACC); Monitor traffic flows Monitor the correct use of agreed traffic routes Recording and logging all incidents. Liaising with stakeholders and emergency services on road and traffic matters Monitoring Temporary Traffic Management Schemes Monitoring traffic management equipment Health and safety obligations Managing site access requirements In the event of an accident, erecting replacement signs, cones, bollards and lights without delay.
Roads General Foreman (Deputy)	TBC	Assist the Traffic Safety and Control Officer
Traffic Management Sub Contractor	TBC	Supply and install warning signs on approach to the development; Management and installation of temporary speed limit signs; Coning management; 2-way traffic sign management; Management of road closures within the project site boundary; and Diversion route management and installation.

6.1.2.1 Cross-Referencing

This plan should be read in conjunction with the following CEMDs:

- Construction Method Statement; and
- Pollution Prevention Plan.

6.1.3 CTMP Revisions and Document Updates

Crucially, this CTMP is a ‘live’ document. It will be reviewed every month throughout all stages of the construction process and in accordance with any changes to the construction works. **This CTMP will be updated with all agreed changes and all revisions and edits logged using the example register shown at the front of this chapter.**

6.1.4 Construction Programme and Calendar

A full description of the construction programme and calendar for activities can be found in the Construction Method Statement (CMS) that is included in the Construction Environmental Management Document (CEMD).

6.2 Personnel and Contact Information

6.2.1 Traffic Safety and Control Officer

Dragados have appointed a Traffic Safety and Control Officer (TSCO) for the duration of the works. The TSCO is responsible for all traffic safety and control operations during the construction works and shall liaise with the appointed construction traffic sub-contractor and all relevant authorities and other stakeholders as required. In the event of any construction traffic issues, please contact the appointed TSCO.

Table 6.2 provides the contact information for the appointed TSCO.

Table 6.2: Appointed TSCO Contact Details

Name	Contact Details
Anthony Gannon	01224 063600
Roads General Foreman (Deputy)	01224 063600

The responsibilities of the TSCO, and of any nominated deputies shall include, but will not necessarily be limited to, the following:

- Manage local permits/consents with Aberdeen City Council (ACC);
- Monitoring, with the assistance of sufficient mobile personnel, the flow of traffic within the area throughout the duration of all construction activities;
- Monitor the correct use of the Coast Road and all other agreed traffic routes for the duration of the construction phase including the restrictions through Torry and Cove;

- Recording and logging all incidents. An ‘incident’ is defined as a shed load, vehicle breakdown, vehicle abandonment, or traffic accident, whether or not the latter involves personal injury;
- Liaising with ACC, Doonies Farm and Emergency services on matters that affect, or are likely to affect, traffic flow through the surrounding local road network;
- Monitoring of all Temporary Traffic Management Schemes associated with the Works;
- Ensuring that all traffic management equipment is in place and in full working order at all times;
- Enforcement of all relevant health and safety obligations;
- Enforcement of site access requirements; and
- In the event of an accident, erecting replacement signs, cones, bollards and lights without delay.

6.2.2 Appointed Traffic Management Sub Contractor

Dragados will appoint a specialised traffic management sub-contractor, prior to the commencement of the construction works. They will complete the majority of construction traffic activities on site and will communicate directly with the appointed TSCO.

Table 6.3 shows the contact details for the Traffic Management Sub Contractor.

Table 6.3: Appointed Traffic Management Sub Contractor

Name	Contact Details
TBC	TBC

The appointed Traffic Management sub-contractor are responsible for managing the following aspects of the CTMP:

- Supply and install warning signs on approach to the development
- Management and installation of temporary speed limit signs
- Coning management
- 2-way traffic sign management
- Management of road closures within the project site boundary
- Diversion route management and installation

The TSCO will be responsible for ensuring that the appointed sub-contractor correctly operate and manage all of their duties with regards to construction traffic measures.

6.2.3 Emergency Contact Details

In the event of an accident occurring in or adjacent to the site, the TSCO or an approved deputy shall immediately contact the emergency services as appropriate. Additional information regarding the response procedure in the event of an emergency can be found in the Dragados Health & Safety Plan.

Table 6.4 summarises all emergency contact details.

Table 6.4: Emergency Contact Details

Organisation / Personnel	Contact number	Non-emergency number
Police Scotland (15 Victoria Road Aberdeen)	999	(101)
Scottish Ambulance Service	999	(01224 812200)
Scottish Fire & Rescue Service	999	(01224 696666)
Construction (site-based) Project Manager	Jose Enrique Perez	01224 063600
Construction (site-based) Health and Safety Officer	David Russel	01224 063600
Coastguard	999	01224 592334

In the event of an emergency, both on and off-site, the TSCO, Construction Health and Safety Officer and Construction Manager must be informed immediately. The TSCO will assist any emergency services on site as required with regards to moving through the site and moving site equipment and personnel to allow uninterrupted access.

6.2.4 Additional Contact Details

The following contacts should be informed of changes to traffic plans that could result in an impact to other road users. It is the responsibility of the TSCO, in conjunction with the Dragados Community Engagement Team, to ensure that details of traffic changes are provided to the contacts in Table 6.5.

Table 6.5: Additional Contact Details

Company/Contact	Phone No.
Community Liaison Manager	08004714829
Aberdeen City Council Media Team/ Karen Allen	01224 522 099
Northsound 1& 2 Radio Stations	01224 337 002 - Newsroom 0141 565 2200- Reception
Original 106 Radio Station	01224 294 860 - Newsroom
BBC Scotland - Aberdeen Broadcasting House	01224 625 233- Telephone:
Roadworks Co-ordination, Aberdeen City Council (Kevin Abercrombie)	01224 523 886
ITS Team, Aberdeen City Council (Neale Burrows)	01224 538 048

6.3 Construction Vehicle Routing

The site is to be well signposted at all times throughout the project with all relevant restrictions shown in advance through the appointed sub-contractor's traffic plan. This plan should include the installation of semi-permanent traffic signs installed on poles and provision of a dedicated customer enquiries telephone number.

The routes in the following sections outline the agreed formal access routes to / from the construction site for different vehicle types. These routes are illustrated in Figure 6.1.

6.3.1 Existing Road Network

The construction site is accessed from the south by Coast Road which crosses a live railway line via a bridge which has a 6.6m wide carriageway. Due to the limited carriageway width, along with the acute 45 degree corners on both the north and south approach roads, the bridge operates on a signalised 'shuttle' basis. While this bridge has no identified weight restriction, Network Rail have confirmed that it can accommodate construction traffic up to 44 tonnes. HGVs currently use this route. The bridge is illustrated in Photograph 6.1 and Photograph 6.2.



Photograph 6.1: Coast Road Railway Bridge (aerial)



Photograph 6.2: Coast Road Railway Bridge, typical HGV movement

The site is also currently accessed from the north-west via St. Fittick's Road which provides connections to Aberdeen City Centre, passing through the residential area of Torry. The function of this road means that on-street parking, bus stops, shops with road-side frontage and a range of traffic calming measures are common along its length. While it is possible to accommodate vehicles such as cars and light goods vehicles (LGVs), it is **not appropriate** for construction-related HGV movements, although it is currently used by HGV traffic.

Throughout the duration of the project a section of Greyhope Road will be closed, restricting vehicle movement along the route. This closure is discussed in more detail in Section 6.5.

6.3.2 Vehicle Routing Management

LGV Access Routes

Light goods vehicles and private cars are permitted to access the site without restriction. If travelling from the south, access through Altens Industrial Estate, Hareness Road and Coast Road should be followed. If travelling from the north or Aberdeen City Centre then the following route should be followed, as illustrated in Figure 6.1:

1. Cross the A956 road bridge over the River Dee and take the first exit from the Roundabout onto South Esplanade West
2. Follow South Esplanade west until its junction with Victoria Road under traffic signal
3. Turn right at the traffic signals and follow Victoria Road in a south easterly direction

The above access route for LGVs and cars has been discussed and agreed with all relevant regulatory bodies.

HGV Access Routes

In accordance with the conditions agreed in the Environmental Statement (ES), all HGVs must approach the site in accordance with the routing details outlined below and as illustrated in Figure 6.1.



Figure 6.1: Agreed LGV and HGV traffic routes

This access route has been agreed and documented with regulatory bodies and must be adhered to at all times.

Under no circumstances will any HGV approach the site by any other route unless fully authorised by the TSCO, Construction Manager and ACC.

Based on the above, the designated route for all construction HGV trips will be as follows:

1. Travel east through the Altens Industrial Estate on Hareness Road from the A956
2. Continue east leaving 2no. roundabouts at their second exits
3. At the priority junction between Hareness and Coast Road, turn north (left)
4. Follow Coast Road in a northerly direction passing over the signal controlled Railway Bridge. Note this structure has a weight limit of 44 tonnes (refer to Photograph 6.1: Coast Road Railway Bridge)

No construction HGVs are permitted to approach the construction site through Torry or Cove. The appointed TCSO and the appointed construction traffic sub-contractor are responsible for ensuring that this construction traffic routing requirement is adhered to at all times. Full routing details should be provided to

all personnel, subcontractors and delivery companies. A declaration to follow these instructions will be included and signed in the driver's log. This includes copies of the drawings presented within Appendix B.

Figure 6.2 presents the traffic island installed at the Grey hope road/St Fitticks junction that will prevent HGV movement from the harbour expansion towards Torry.

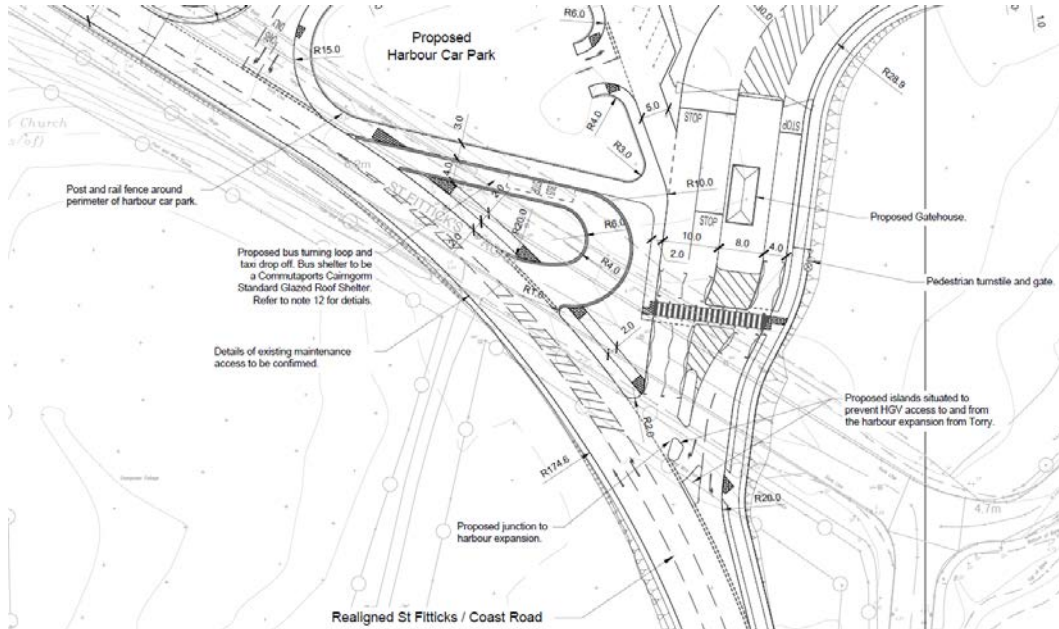


Figure 6.2 Road Layout drawing highlighting the proposed traffic islands

The appointed construction traffic sub-contractor is responsible for establishing all necessary traffic management measures to accommodate construction traffic using Coast Road and agreeing measures with the transportation officers of ACC. The traffic management signage scheme includes signage on St Fittick's Road that must be adhered to at all times, alerting project HGVs that travelling through Torry is not permitted.

Appendix B2 contains the permitted access routes for all HGV construction trips when accessing or egressing the site Compounds.

6.4 Contractor Compound Traffic Management

6.4.1 Compound Locations

There are three construction Compounds each receiving different levels of HGV traffic and material deliveries. The TSCO is responsible for liaising with the Construction Manager and any other relevant individuals to identify which HGVs will be accessing the different Compounds.

A brief description of each Compound can be found below:

6.4.2 Southern Compound

The Southern Compound (Figure 6.3) is located to the south of Nigg Bay and is bounded by the Coast Road to the west and the coastal slope to the north and east. The Compound will be used primarily for the fabrication and storage of accropodes.

The Compound will include:

- Office and welfare cabins (total plan area 15m x 15m);
- Laboratory for concrete testing with a plan area of 15m x 15m;
- Concrete batching plant: which comprises the aggregate storage area (35m x 15m) and the plant itself including the mixer, 6no aggregate silos and 4no cement silos which has an overall footprint of approximately 42m x 22m. The batching plant, including silos will sit on reinforced concrete footings;
- Fabrication shed: a temporary structure 115m x 38m within which the accropodes will be fabricated;
- Pre-stock area: a 30m x 90m used during the curing cycle of the accropodes; and
- Accropodes storage area: the remaining areas of the Compound will be levelled for storage of completed accropodes prior to placing in the breakwaters.

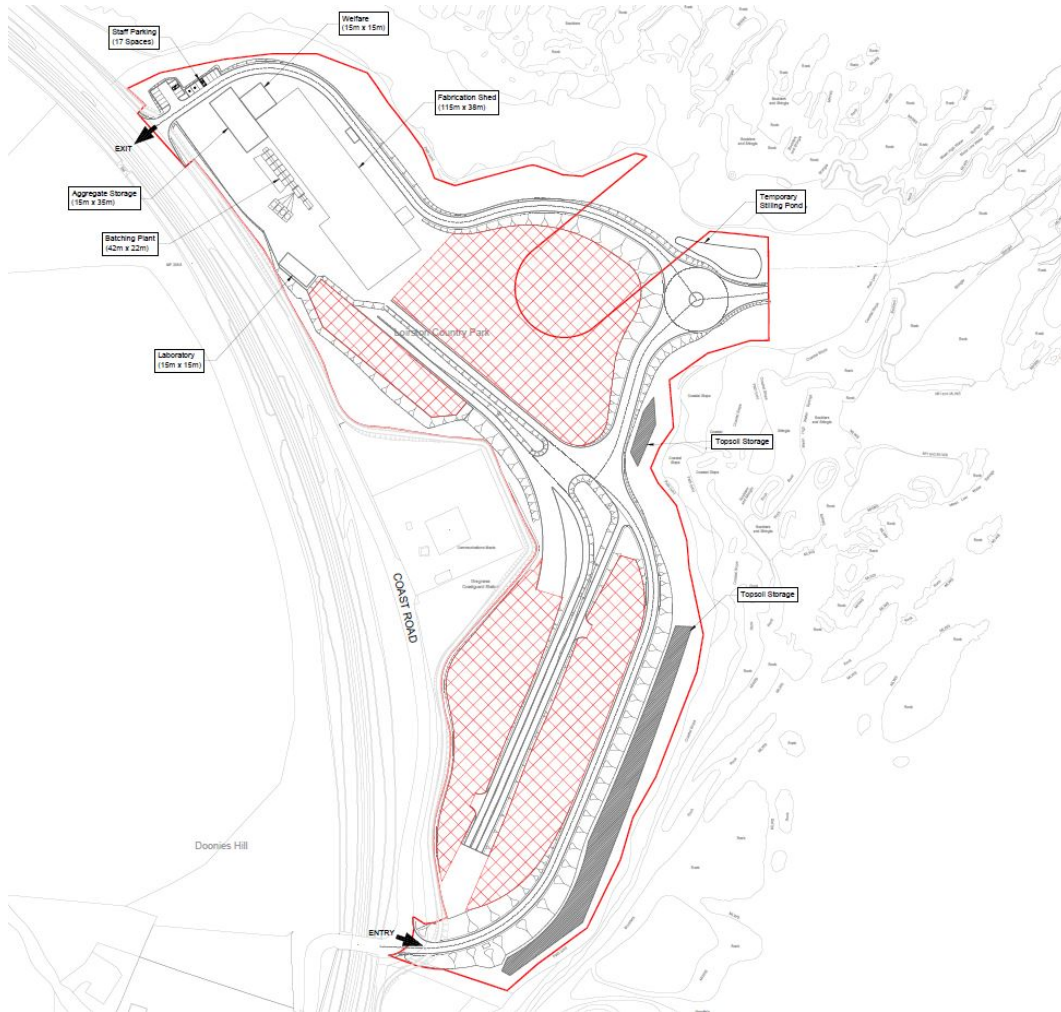


Figure 6.3: Southern Compound Site Layout

Access and exit to the Southern Compound is along the Coast Road at different locations. These points will be managed by appointed traffic marshals to ensure controlled movement into the site.

6.4.3 Northern Compound

The Northern Compound (Figure 6.4) is located at Walker Park and is fully enclosed by the existing stone wall. It will be accessed from Greyhope Road to the south. The Compound will comprise:

- Storage, warehouse and temporary fabrication area located to the north of the site. The overall area is 100m x 60m and will incorporate single storey temporary warehouses (40x12m) and stores created from metal portacabins installed on mass concrete bases. This area will also be used temporarily to fabricate 8m³ accropodes;
- Storage Expansion located at the centre of the Compound. Dimensions approximately 140m x 70m. This open area will be used to store different materials at the different works stages (aggregate, rock, reinforcement, plant, etc). This area will be compacted hard standing;
- Car parking spaces for up to 30 cars; and

- Temporary canteen and toilet facilities provided as mobile units.

Access will be provided from Greyhope road with an asphalt access road within the Compound. This will be managed by an appointed traffic marshal who will ensure controlled movement into the site. This section of Greyhope Road will be closed during the construction phase.

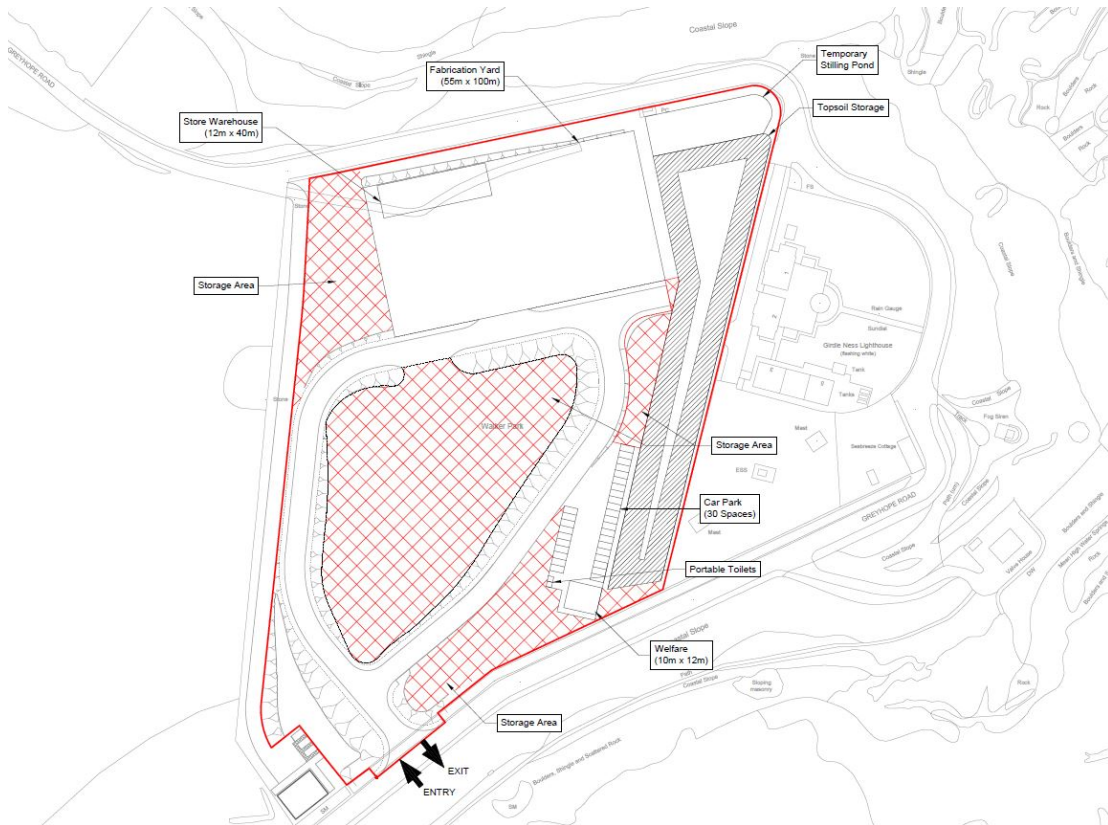


Figure 6.4: Northern Compound Site Layout

6.4.4 Central Compound

The Central Compound (Figure 6.5) is split into two parts, separated by St Fittick's Road.

To the north, an area of approximately 8500m² will be used for welfare facilities, project offices, car park and plant storage. The majority of the site will have an asphalt surface.

To the south of St Fitticks Road, an area of approximately 26,800m² will be used for storage of materials including accropodes, aggregates, reinforcement, plant etc. The Compound will be finished with compacted hard-standing.

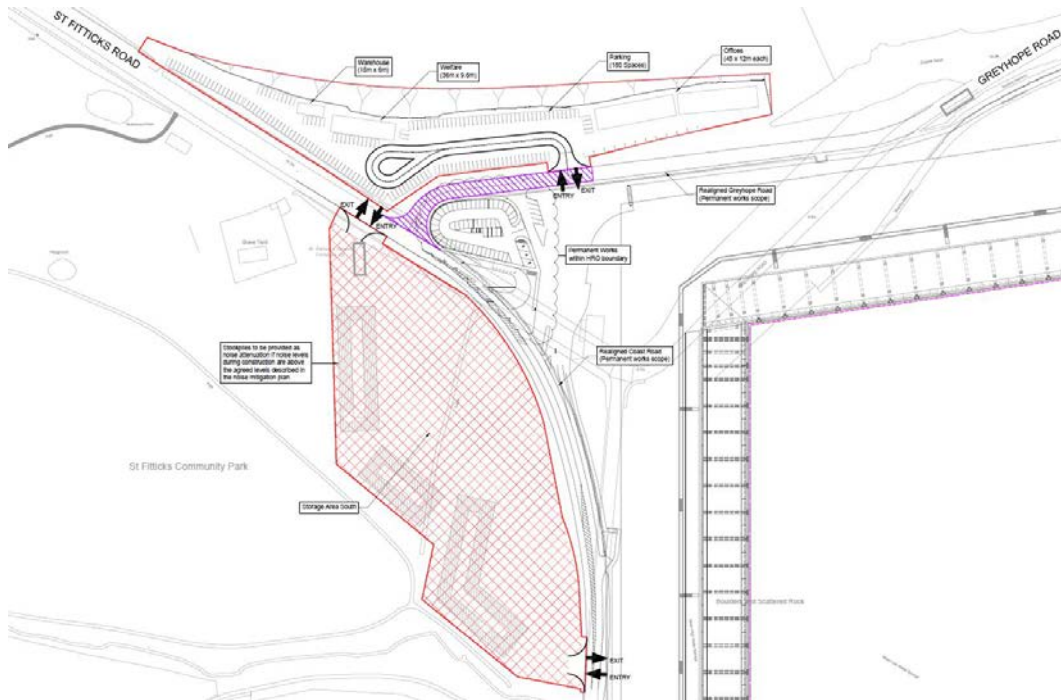


Figure 6.5: Central Compound Site Layout

There are three access/exit points to the Central Compound. These points are will be controlled by an appointed traffic marshal for the Compound.

6.4.5 Compound Management and Site Vehicle Operations

Throughout the duration of the works, the following key principles will be established within the compound sites:

- A clearly identified staff pedestrian route through the construction site, segregated with fencing and / or white lining;
- No loud radios or engine idling after 18.00;
- All HGVs to have white noise reverse warnings;
- Clear parking instructions and zoning for staff and visitor cars;
- Clear segregation and signage for material storage points within the site;
- A lorry 'stop over' area will be provided within each site with signage erected to direct HGV traffic to the location;
- 'Give Way' signs will be erected for traffic leaving the site;
- All site access will allow all vehicles to enter the site in a forward gear. In the unlikely event that reversing is required, a competent banksman will be in attendance at all times;
- A temporary speed limit of 10mph will be implemented for all vehicles travelling within any of the construction sites for the duration of the construction works;

- The site will be laid out to provide marshalling areas for vehicles, such that off-site holding areas will not be required;
- Deliveries will be scheduled with the main suppliers to ensure that overnight parking takes places in organised areas; and
- Construction vehicles will avoid travelling in convoys on public roads.

Access to the Northern Compound is via Greyhope Road. Access to the Southern and Central Compound is via either the Coast Road or Greyhope Road.

The TSCO and Construction Manager will be responsible for ensuring all deliveries to and from the site are managed effectively, reducing traffic volumes and unnecessary disruption to the operation of the existing road and transportation networks. Reviews of the site Compound traffic measures will be carried out every two months and adjustments to the existing measures will be implemented into the plan.

6.4.6 Site Parking

Construction staff using private vehicles to travel to the site will park their vehicles in designated construction site car parks in the three Compounds described above. Parking of vehicles being used for construction purposes is not permitted on public roads.

The Compounds have the following number of designated car parking spaces

- Northern Compound (30 spaces);
- Southern Compound (17 spaces); and
- Central Compound (130 spaces).

Temporary car parking arrangements will be made at the start of the Enabling Works. More specifically:

- Construction staff using private vehicles to travel to the site will park their vehicles in designated site car parks and not on public roads within a two mile radius of the site
- Parking of vehicles being used for construction purposes is not permitted on public roads to minimise disruptions to traffic and local residents

6.4.7 Deliveries to Site

Deliveries for most materials will occur 24 hours a day but will be organised to avoid 'peak road activity' times between 08.00-09.30 and 16.00-17.30. Deliveries will be scheduled to occur at specific times to maintain the safe and efficient operation of the construction works. The delivery of stone for the breakwater construction is required 24 hours a day, 7 days a week.

6.5 Traffic Management Procedures

6.5.1 Company Log

A log of organisations travelling to site will be maintained within the project site office and will include a record of agreements with organisations to demonstrate their understanding of the prescribed access routes. It is the responsibility of the TSCO to manage this log and to ensure that the agreed routing agreements are adhered to by all project staff.

If the log is not adhered to, the subcontractor or supplier is in breach of contract, allowing disciplinary action against individual drivers. The log to be used is included in Appendix A.

6.5.2 HGV Windscreen Markers

HGVs that are associated with the project will be provided with a marker and unique number that must be displayed on their windscreens. This will indicate which HGVs are associated with AHEP and should therefore comply with the agreed routes, as described in Section 6.3. It is the responsibility of the TSCO to ensure that HGV numbers and markers are distributed to all HGVs and to ensure the HGV drivers are aware that numbers should be visible at all times.

6.5.3 Temporary Traffic Management Schemes

All traffic management works are managed by the appointed construction traffic sub-contractor.

All Temporary Traffic Management Systems (TTMS) will be designed and installed in accordance with the design standards specified in the Environmental Statement Chapter 18: Traffic and Transport as well as the following Department for Transport guidance documents:

- Traffic Signs Manual, Chapter 8 (part 1) road works and temporary situations – design (2009)
- Traffic Signs Manual, Chapter 8 (part 2), road works and temporary situations – operations (2009)

All TTMS should also take cognisance of the 2013 DfT guidelines, ‘Safety at Street Works and Road Works: A Code of Practice’. A risk assessment and method statement is required to be completed for the installation of each TTMS and will consider hazards associated with each scheme and any necessary control measures. It is the responsibility of the TSCO to ensure that all risk assessments and method statements are completed and kept up to date.

For each TTMS a specific Traffic Management Plan is required, including drawings that provide the detailed equipment requirements (i.e. cones, temporary signage, temporary traffic lights, etc.). Appendix A contains the proforma which is to be completed for individual Traffic Management schemes. Further key items that must be addressed within each Traffic Management Plan are as follows:

- Temporary vehicular, pedestrian and cycle diversions shall be provided where relevant
- Consultation with any relevant public transport operators regarding traffic management schemes will take place through a Traffic Management Working Group (TMWG) forum (refer to Section 7.4)
- Bus routes affected by the works will be described in the specific Traffic Management Plan
- Private accesses affected by the TTMS shall be maintained unless otherwise agreed in writing with the owner / occupier and Dragados

Signage Schemes

To ensure that the TTMS are followed effectively by all staff, a series of signs are required to be placed at various points around the site to inform users of the TTMS being implemented. It is the responsibility of the TCSO and the appointed sub-contractor to ensure that all signage plans are completed and maintained throughout the duration of the TTMS.

All signage scheme drawings for each TTMS will be recorded and stored in Appendix B. These schemes are to be implemented during the early stages of the Enabling Works. This scheme will involve signage at the following locations:

- [Drafting note: Info to be added once sub-contractor appointed]

The signs will be maintained through the Contract and the need for additional signage will be kept under review by the TSCO. The TSCO is responsible for ensuring these layouts must be adhered to and maintained throughout the duration of works.

Dragados will power signs within close proximity of the project site using the electricity supply from the compounds. If required, Dragados will also provide electricity to the relevant signage further from the project site.

Speed Limit Reductions

In accordance with the agreed conditions in the Environmental Statement (ES) Appendix 18-A, speed limit reductions are to be implemented along Coast Road between the site and the Hareness Road junction. A temporary speed limit of 40 mph is to be imposed on the Coast Road south of the railway bridge, and 30mph north of the railway bridge to St Fitticks Church.

All speed limit reductions will be implemented through the promotion of Temporary Traffic Regulation Orders (TTRO) that Dragados must apply for at least 30 days prior to the dated implementation. It is the responsibility of the TSCO and Dragados to ensure that all TRROs are implemented correctly. The guidelines for implementing a TTRO can be found on the Transport Scotland website: <http://www.transport.gov.scot/road-and-traffic-orders>

If future speed limit reductions are required, it is the responsibility of the TSCO and the appointed traffic management sub-contractors to identify and agree with

officers of ACC those areas where speed limit reductions are required. The TSCO and appointed sub-contractor will then be responsible for the preparation and submission of all TTROs to ACC for their consideration.

Figure 6.6 shows the temporary speed reduction layouts which have been developed as a mitigation measure and described in the Environmental Statement (ES). Additional limit restrictions must also be shown in the same layout format and stored in Appendix B. These layouts must be adhered to throughout the entire duration of the project.

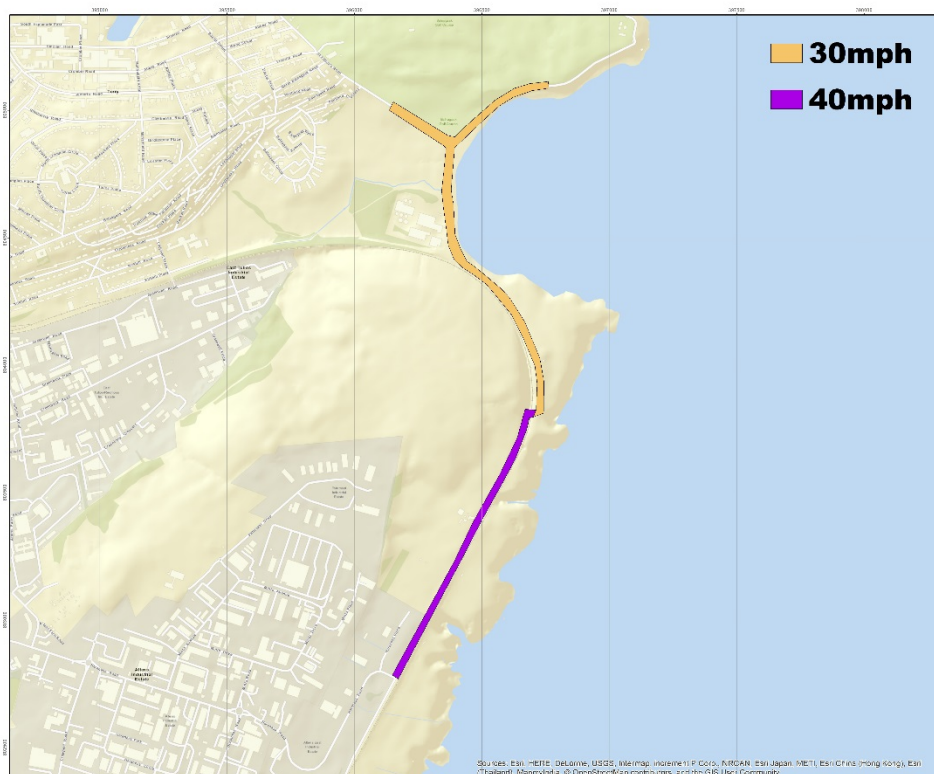


Figure 6.6: Temporary Speed Reduction Scheme(s)

Road Closures

A temporary closure of Greyhope Road during the construction phase for a distance of approximately 1km will be implemented between Coast Road and the track to the Valve House access track. This is to allow large construction vehicles to access the northern breakwater and the temporary working area at Walker Park (refer to Appendix B).

The closure will begin at the junction between Greyhope Road and St Fittick's Road and will continue along Greyhope Road until approximately 20 m prior to the Valve House access track. A layout of the closure can be seen in Figure 6.7.

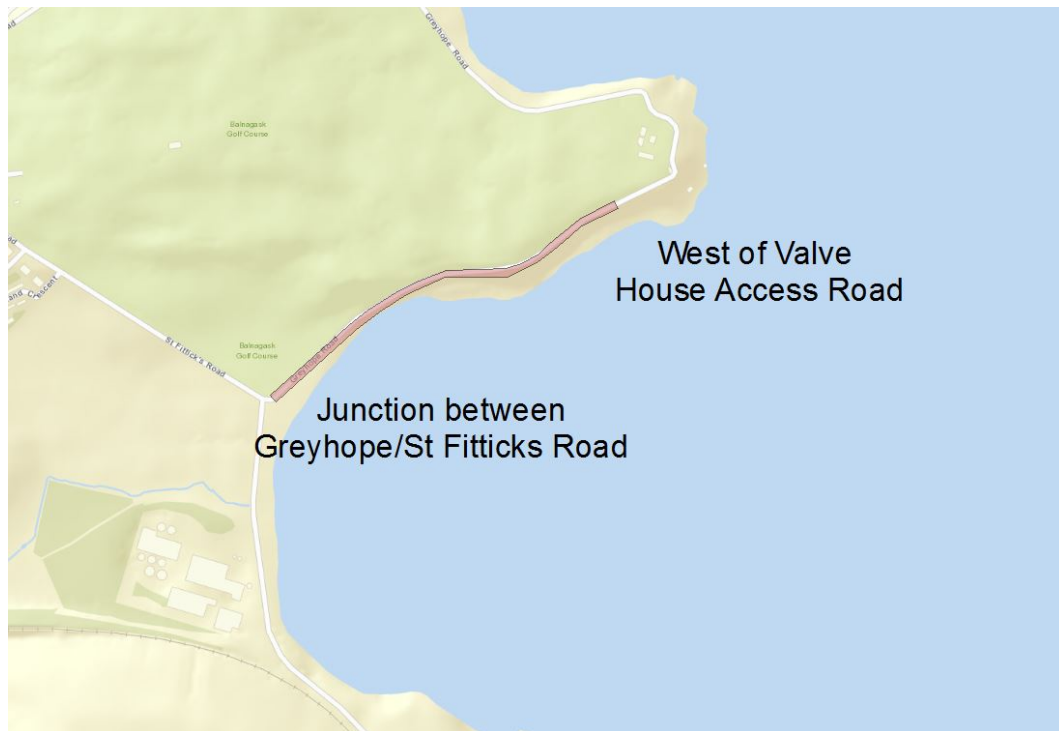


Figure 6.7: Greyhope Road Closure Locations

Throughout the course of the construction, access to the residential properties at Girdleness headland will be retained along the northern section of Greyhope Road. Signage on this route will inform drivers of the temporary road closure ahead. Signage will also inform drivers that the RSPB Dolphin watch group is still operating in the Torry Battery car park and drivers wishing to meet must do so by driving from the north side of Greyhope Road.

The TSCO and appointed sub-contractor will apply for all TTROs which are required for the temporary closure of specific roads and will prepare any proposal for the temporary closure of any road not already agreed with ACC. The TSCO is responsible for ensuring that any closures will comply with requirements from ACC and have appropriate legal status via the promotion of a TTRO. Potential future road closures must also follow this procedure if required.

Established Clearway

Throughout the duration of the construction phase of the project, a traffic clearway will be established between the Coast Road railway bridge and the Torry St. Fittick's Parish Church ruin. This is to ensure that trade to the temporary / mobile 'fast food' outlets are unaffected by the clearway restrictions. While the clearway is operational, no vehicles are permitted to stop along the route, including vehicles loading / unloading or waiting. Such restrictions will be the subject of a TTRO. Figure 6.8 illustrates the extent of the proposed clearway, however, specific details of the length of road over which the clearway will be implemented will be contained within the TTRO.



Figure 6.8: Extent of Clearway along Coast Road and St. Fittick's Road

It is the responsibility of the TSCO to ensure that the drivers of all construction vehicles are aware of the restrictions imposed by the clearway. Enforcement of the TTRO under which the clearway will be implemented will be the responsibility of Police Scotland.

Temporary Traffic Lighting

Temporary traffic lights will be established at the entrance/exit points of the Southern Compound site for vehicles moving onto Coast Road during times of significant traffic activity.

It is the responsibility of the TSCO to ensure that these traffic lights are used during appropriate periods of significant activity and all vehicles use the temporary traffic lights correctly.

It is crucial that appropriate signage is erected on the approach to all temporary traffic lights to warn drivers of potential queues and / or the need to stop. Such signage will be erected in accordance with all relevant Department for Transport guidance documents, as outlined in Section 6.5.

The temporary traffic lights will be located to ensure that they are visible to drivers within sufficient stopping distances. Furthermore, the warning signage will work in conjunction with the proposed temporary speed limits which will be in place along Coast Road throughout the construction period.

Banned Manoeuvres

The following vehicle manoeuvres are banned on public roads by all construction vehicles:

- Three- Point Turns; and
- U- Turns.

Any construction vehicles wishing to complete these manoeuvres must do so within the construction compound sites. HGV vehicles are also prohibited from turning on to St Fittick's Road.

6.5.4 Abnormal Vehicle Requirements

Construction traffic movements will include deliveries which may, depending on their characteristics, be classified as an abnormal load. They can be delivered by road or potentially by sea to the existing harbour and transported to site.

In accordance with Police Scotland guidelines, an 'abnormal' load is a vehicle that meets any of the following criteria:

- A weight of more than 44,000kg gross weight;
- An axle load of more than 10,000kg for a single non-driving axle and 11,500kg for a single driving axle;
- A width of more than 2.9m; and
- A rigid length of more than 18.65m, or an overall length of more than 25.9m.

Permission for the transport of abnormal loads is required from the ACC Roadworks Co-ordination team prior to the loads being transported on the public road network. Furthermore, Police Scotland are required to be notified of these movements, but permission is not required from this organisation. Contact details for both organisations are outlined in Table 6.6.

Table 6.6: Abnormal Loads Contact Details

Organisation	Requirements	Contact Details
Roadworks Co-ordination team, Aberdeen City Council (Kevin Abercrombie)	Permission	01224 523886
Abnormal Loads Administration Team, Police Scotland	Notification	AbnormalLoadsScotland@scotland.pnn.police.uk 01592 418859

The limited weight restriction of 44 tonnes on the Coast Road Rail Bridge must not be exceeded by vehicles travelling this route. There are no construction activities which are expected to require such abnormal vehicle movements. In the event of a load that exceeds this weight and / or length being transported, special permission to use the northern route must be obtained by the TSCO.

It is the responsibility of the TSCO to ensure that the movement of abnormal loads are managed correctly.

Under the New Roads and Street Works Acts (NRSWA) 1991, Symology notices are required to notify statutory undertakers of road works or closures. It is the responsibility of the TSCO and their appointed deputy to ensure that all notices are issued correctly.

6.5.5 Railway Interface and Consent Procedure

In event of alterations to roads or paths which will impact with railway infrastructure, the TSCO will consult with Network Rail and obtain any consents necessary for the works to be undertaken. This will also apply should line access be required.

6.5.6 Mud and Dust Control Procedures

Mud and Wheel Washing Facilities

In accordance with the Scottish Environment Protection Agency (SEPA) guidelines discussed in the Pollution Prevention Plan, a wheel washing facility will be established in all three site Compounds. These facilities will be located at a site access / egress junction in each Compound site. A temporary wheel washing facility will also be available during the enabling works.

Works and vehicle paths within the site should be arranged to minimise the potential for soil to be picked up by vehicle wheels.

The TCSO is responsible for maintaining the operation of the wheel washing facilities and will inspect the equipment on a weekly basis.

Dust Control

In accordance with the Pollution Prevention Plan, specific controls relating to the avoidance of dust for vehicles entering and leaving the site will be monitored by the TSCO and will include the following measures:

- Maintenance of all haul roads and hardstanding site areas by regular brushing and water spraying;
- All vehicles carrying soil and other dusty materials to be fully sheeted;
- Enforcement of site speed limits;
- A daily sweeping regime of the main access road within the site and along Coast Road and St Fittick's Road during the construction period will be established and managed by the TSCO; and
- To minimise the likelihood of ice forming on the road surface, water spraying during periods of cold weather will not be permitted.

6.6 Pedestrian and Cycling Routes

6.6.1 Aberdeen City Council Core Path

All staff must be cognisant of cyclists, pedestrians and other vulnerable road users when planning, programming, undertaking and monitoring all construction works.

Construction traffic will not be routed along St Fittick's Road through the residential areas of Torry and Balnagask, which are part of Core Path no. 104.

6.6.2 Temporary Routing

The route of the existing coastal and cycle path will be altered during the construction phase. These adjustments will result in a continuous coastal footpath and cycle path throughout the duration of the construction phase. The alternative route during the construction period is shown in Figure 6.9. These routes will be signposted and managed by traffic marshals who will ensure that construction traffic from each gate in the southern and central Compounds will not interfere with the temporary routes.

Signage alerting construction staff of the cycle path will be erected around the project site.

The coastal and cycle path will be rerouted within the verge on the east side of Coast Road adjacent to the site. A 2m wide footway will be constructed with the surfacing to be agreed with ACC.

The TSCO and appointed traffic sub-contractor will manage the route of the coastal and cycle path to ensure that safe passage is available throughout the course of the construction phase. Over the duration of the project the route will vary around the Coast Road/St Fittick's Road/Greyhope Road junction to ensure pedestrian/cyclist amenity and safety as site access works progress.

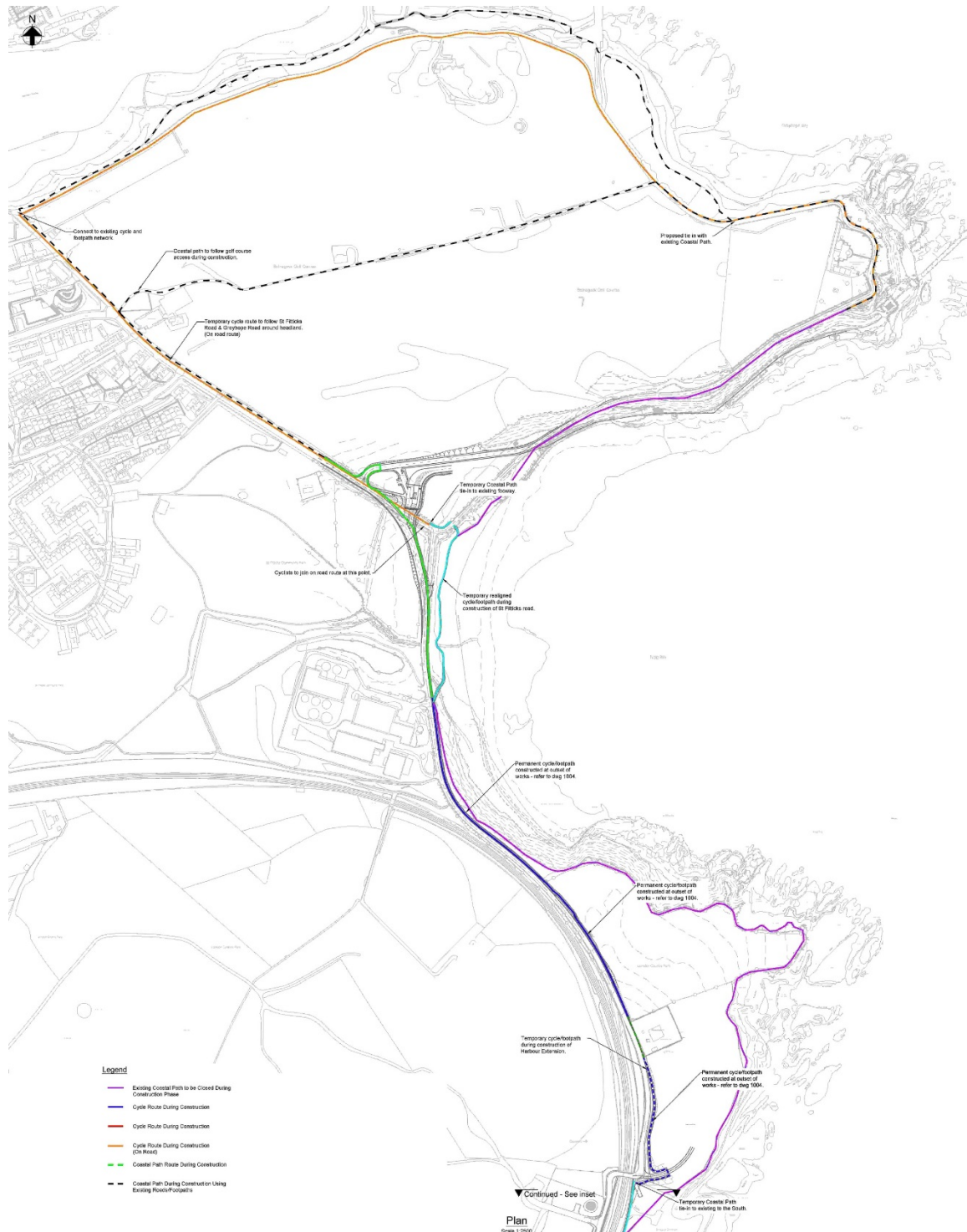


Figure 6.9: Proposed Coastal Path Temporary Alignment

Source: Arup

6.7 Monitoring Strategy

6.7.1 Overview

As part of the ongoing process for ensuring that any impacts on the operation of the surrounding transportation networks due to construction traffic are minimised, a CTMP monitoring strategy will be established between Dragados and ACC. The named TSCO will be responsible for managing all monitoring activities and associated reporting. A six monthly report shall be prepared and presented at a Traffic Management Working Group (TMWG). Refer to Section 6.7.4 for more details.

6.7.2 Construction Traffic Inspection

In accordance with the Planning Permission in Principle (PPiP), a condition survey will be completed before and after the construction phase on the Coast Road to observe the effect of construction traffic on the public highway network, and at any agreed intervals in between. The Section 69 Agreement between ACC and Aberdeen Harbour Board has identified that the survey encompasses the Coast Road between Hareness Road and Nigg Bay. Sections of Greyhope Road may also be incorporated.

The methodology and, where appropriate, the frequency of the condition surveys will be undertaken in accordance with the Section 69 Agreement and any subsequent discussions and agreements with ACC. Any questions or complaints received regarding construction traffic will be logged, investigated and resolved using the environmental plan external complaints policy. It will be the responsibility of the TSCO to monitor the construction traffic surveys, manage the complaints and, where relevant, delegate specific tasks to other suitably qualified and experienced contractor staff.

Contact numbers for members of the public to register complaints will be on signage placed around the project site, on the AHEP and AHB websites, provided within the quarterly project newsletter and available when the project team regularly attend the Cove & Altens and Torry Community Council meetings.

6.7.3 Plan Review

The contents of this CTMP will be reviewed every two months by the TSCO and updated when appropriate. The CTMP register, as contained within Appendix A, must be updated by the TSCO to reflect any changes and revisions to this document.

6.7.4 Traffic Management Working Group

A Traffic Management Working Group (TMWG), chaired by the Construction Traffic Safety Officer or an appointed deputy, will be formed for the entire construction period of the project and will include representatives from the following organisations and stakeholder groups:

- Aberdeen City Council;
- Aberdeen Harbour Board;
- Police Scotland;
- Torry Community Council;
- Cove & Altens Community Council; and
- Kelda Water.

Other stakeholders will include, where relevant, representatives from Nigg Bay Golf Club, cycle interest groups and Transport Scotland (as trunk road operator).

The TCSO is responsible for organising the meetings on a bi-monthly basis and will inform members of the TMWG about forthcoming traffic management proposals. Information to be discussed at the meetings shall be issued 7 days in advance of the next meeting. This shall include details on the monitoring process and identification of any specific issues that need to be addressed.

6.8 Additional Projects

All construction and transportation staff should be aware of the following projects which may also cause additional activity on the road networks that will be used when travelling to the project:

- Aberdeen Western Peripheral Route; and
- Morrison's/SITA UK Recycling Plant.

It is the responsibility of the TSCO to ensure that all relevant staff are aware of the projects and the significant impacts they may have on project traffic routes.

Particular cognisance should be given to the interaction between all traffic management activities and any similar traffic management schemes which relate directly to the AWPR project, particularly south of the site and within the vicinity of the A90 / A956 interchange. The TSCO is responsible for liaising with the AWPR Temporary Traffic Manager to ensure that the impact of construction vehicle movements is minimised. Contact details are provided in Table 6.7.

Table 6.7: AWPR Contact Details

Name/Role	Contact Details
Mark Neville Temporary Traffic Management Officer- AWPR Construction Joint Venture	Office Telephone : 01224 258300 Mobile Telephone : 07789 925309 Email : Mark.Neville@awprcjv.com

Appendix A

Registers

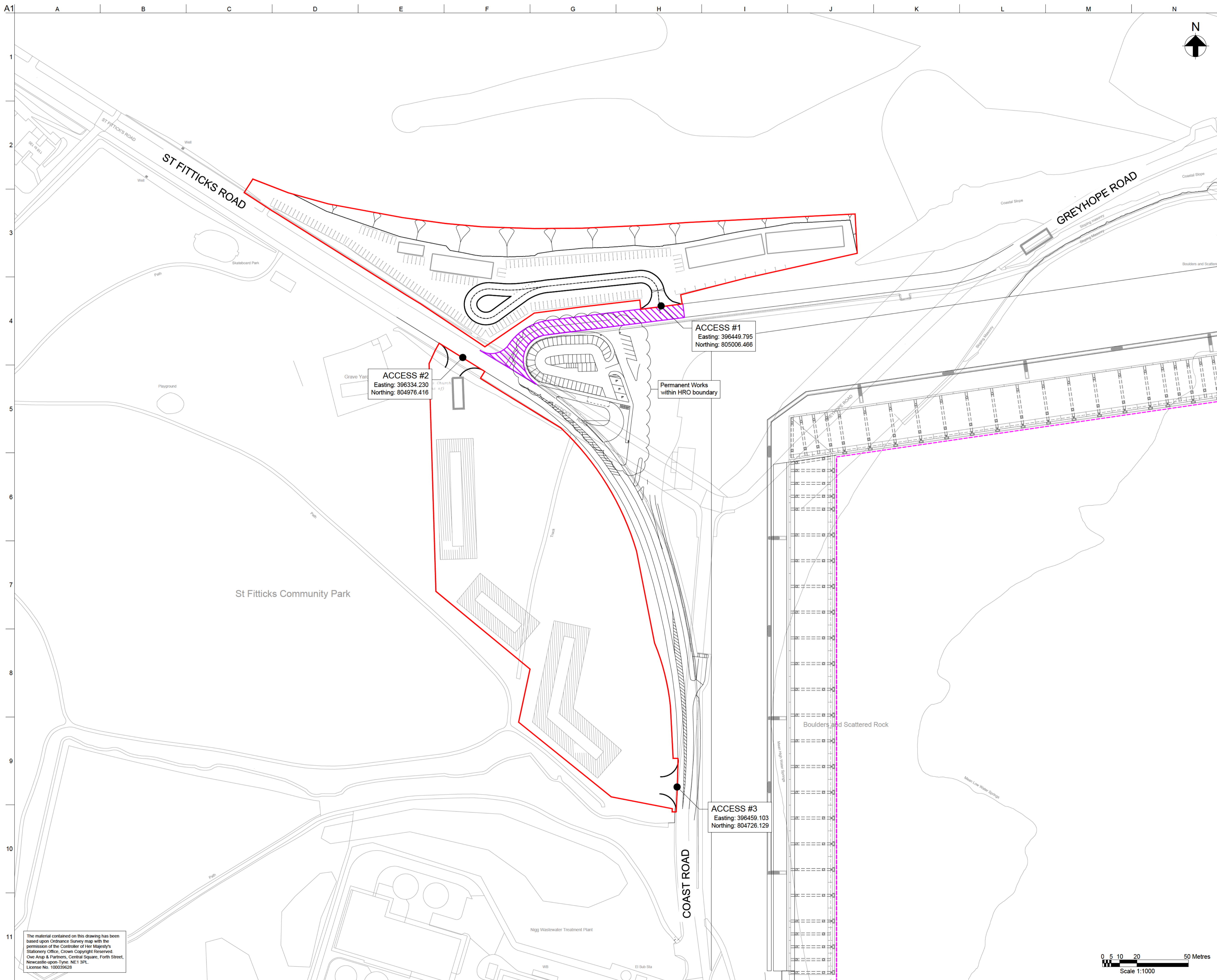
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Appendix B

Technical Drawings

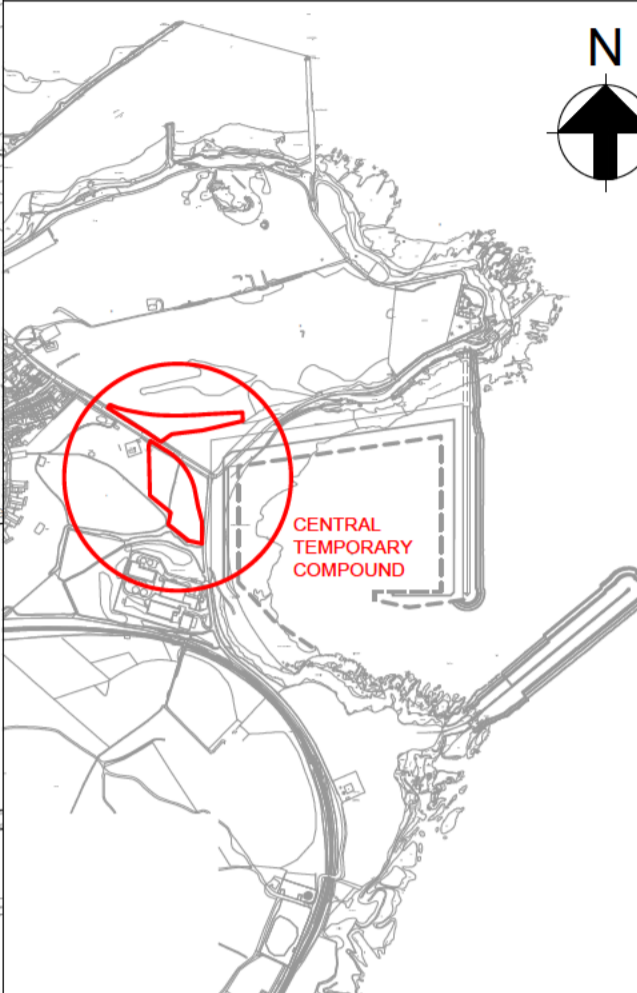
B1 Compound Layouts



HEALTH AND SAFETY INFORMATION

SIGNIFICANT OR EXCEPTIONAL RISKS ARE IDENTIFIED BELOW

(Insert Risks Here)



Key Plan

Legend:

 Site Boundary

* To be phased with Greyhope alignment works

 Central Access Road

0	15/02/17	MWM	RF	NL
Issued For Information				
Issue	Date	By	Chkd	Appd

ARUP

Scotstoun House, South Queensferry
West Lothian, EH30 9SE
Tel +44 (0)13 1331 1999 Fax +44 (0)13 1331 3730
www.arup.com

Client



Job Title
Aberdeen Harbour
Expansion Project

Drawing Title

Central Temporary Compound Site Location Plan

Arup Job No 253300-00	Scale at A1 1:1000
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Discipline 00 - General Drawings

Drawing Status

Issued For Information

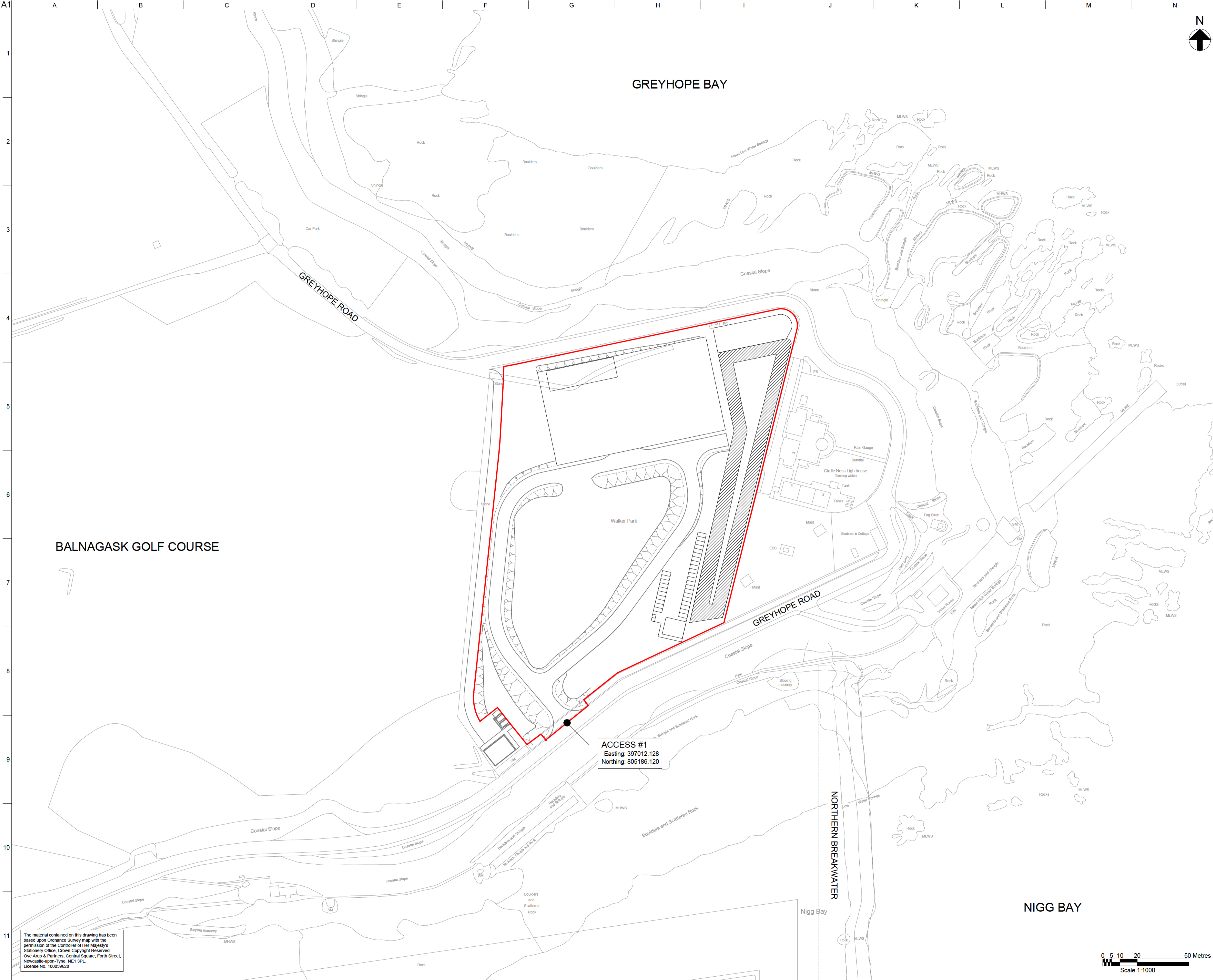
Drawing No.

DRA-T-CEC-TW0001-DWG-000000-000

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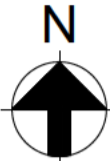
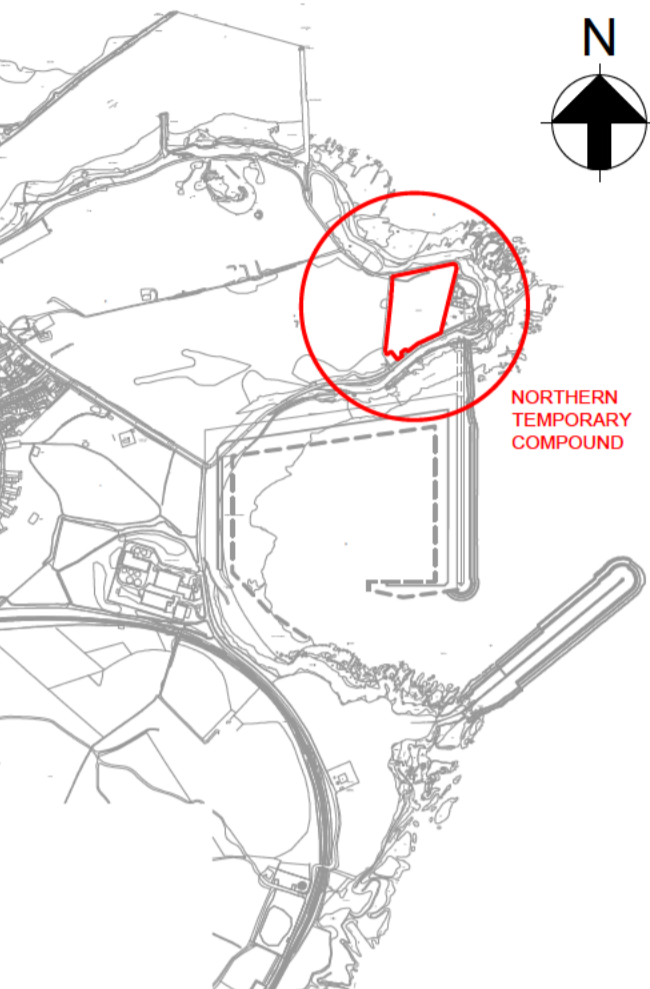
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HEALTH AND SAFETY INFORMATION

SIGNIFICANT OR EXCEPTIONAL RISKS ARE IDENTIFIED BELOW

(Insert Risks Here)



NORTHERN
TEMPORARY
COMPOUND

Key Plan

Legend:

Site Boundary

0	17/02/17	MWM	RF	NL
Issued For Information				
Issue	Date	By	Chkd	Appd

ARUP

Scotstoun House, South Queensferry
West Lothian, EH30 9SE
Tel +44 (0)13 1331 1999 Fax +44 (0)13 1331 3730
www.arup.com

Client

DRAGADOS

Job Title
**Aberdeen Harbour
Expansion Project**

Drawing Title
**Northern Temporary Compound
Site Location Plan**

Arup Job No **253300-00** Scale at A1 **1:1000**

Discipline **00 - General Drawings**

Drawing Status **Issued For Information** Rev **0**

Drawing No **DRA-T-NOC-TW0001-DWG-000000-000**

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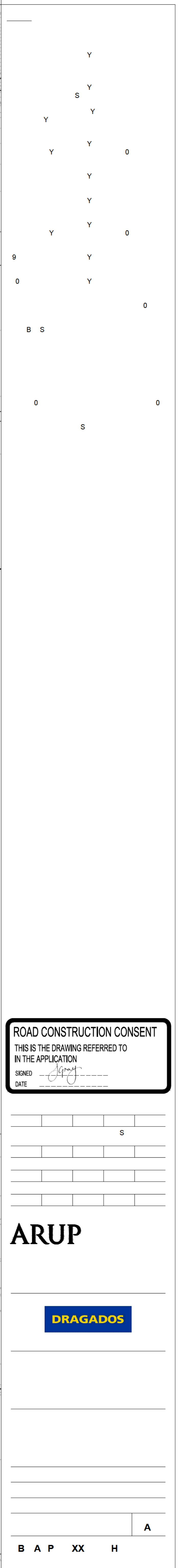
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B2 Road Layout



B3 **Traffic Signage Schemes**
