

Licensing Operations Team  
Marine Directorate  
By email

Our ref 683240/JSH/003  
Telephone [Redacted]  
E-mail [Redacted]

18 February 2026

Attn: Marine Directorate Licensing Operations Team (MD-LOT)

**Engineering Works to Form New Eastern Inner Dock Quay, Port of Nigg - Marine Construction Licence (MS-00010859)  
Request for Variation [Doc. Ref 16012 Issue 1]**

We write on behalf of our clients, Global Energy Nigg Limited, regarding the marine construction licence (MS-00010859), which authorises the works to modify the existing rock revetment forming the east side of the inner dock to form heavy-duty quayside faced with a vertical retaining wall to primarily facilitate the export of High Voltage (HV) cable manufactured at the adjacent Sumitomo Electrics factory directly onto cable installation vessels and also serve as an additional facility to support the existing operations at the site.

This correspondence relates to the following items:

- A. Notification of variation, in accordance with Part 1, Section 1.4 of the marine construction licence stated above, to the location of the licensed activity as set out in Part 2, Section 2.2 of the same licence;
- B. Notification of variation, in accordance with Part 1, Section 1.4 of the marine construction licence stated above, to reuse recovered rock armour stone at both ends of the new quay; and
- C. Formally request a screening opinion under The Marine Works (Environmental Impact Assessment) (Scotland) Regulations 2017 for the matter covered under points A and B above.

The attached appendices provide further information for each of the items listed above. If you have any questions relating to the information provided, please do not hesitate to contact the undersigned.

Yours sincerely  
**for EnviroCentre Limited**

(issued electronically)

**Jeet Shah**  
Senior Environmental Consultant

**Campbell Stewart**  
Technical Director

Enc: Marine Construction Licence Variation Application

Environmental Impact Assessment (EIA) Screening Request  
**A Drawings**

CC:

# MARINE CONSTRUCTION LICENCE VARIATION APPLICATION

## Introduction

1. As you will be aware, Global Energy Nigg Limited is committed to upgrading the existing rock revetment forming the east side of the inner dock of Port of Nigg to a heavy-duty quayside faced with a vertical retaining wall to facilitate the export of High Voltage (HV) cable manufactured at the adjacent Sumitomo Electrics factory directly onto cable installation vessels, and also serve as an additional facility to support the existing operations at the site. These changes are detailed on Drawings 683240GIS004 – detailing revised boundary and EIDQ-RWC-00-XX-DR-C-00004, EIDQ-RWC-00-XX-DR-C-00040, EIDQ-RWC-00-XX-DR-C-00045 in Appendix A. To be able to achieve this goal, the following items need to be considered.
2. a) Alternation to the existing licensed site boundary; and
3. b) Inclusion of the placement of recovered rock armour at both ends of the new quay.
4. Part 1, Section 1.4 of the marine construction licence states the following.

*“Under section 30 (7) of the 2010 Act, on an application made by a licensee, the Licensing Authority may vary a licence if satisfied that the variation being applied for is not material.”*

We therefore provide information on the proposed changes to the site boundary and reuse of recovered rock armour at both ends of the new quay and present information to demonstrate that the changes do not constitute a material change as referred to in Part 1, Section 1.4 of the marine construction licence.

## Site Boundary

5. The project is subject to a design and build contract, and as such, as part of the detailed design stage, an alteration to the existing site boundary is required to allow for changes to the layout of the works and associated boundary detailed on the existing licence. The changes required to allow for the inclusion of the changes to the bunds associated with the temporary works. The new coordinates required to achieve this boundary are detailed below. The alternation is a slight adjustment to the existing site boundary, as shown in the Appendix **Error! Reference source not found.**. This will result in less than 1% increase in the site area from 23,284 m<sup>2</sup> to 23,380 m<sup>2</sup> associated with the temporary works element only, bund to facilitate piling, and does not impact the footprint of permanent elements of the construction project. The original and the proposed site boundary point coordinates are provided in Table 1 below.

**Table 1: Revised Boundary Coordinates**

S/N	Original Coordinates		Revised Coordinates	
	Latitude	Longitude	Latitude	Longitude
1	57° 41.793' N	04° 01.511' W	57°41.697'N	004°01.540'W
2	57° 41.857' N	04° 01.519' W	57°41.704'N	004°01.563'W
3	57° 41.884' N	04° 01.608' W	57°41.824'N	004°01.577'W
4	57° 41.864' N	04° 01.641' W	57°41.832'N	004°01.627'W
5	57° 41.830' N	04° 01.637' W	57°41.857'N	004°01.635'W
6	57° 41.827' N	04° 01.581' W	57°41.885'N	004°01.614'W
7	57° 41.792' N	04° 01.576' W	57°41.871'N	004°01.598'W
8	57° 41.740' N	04° 01.570' W	57°41.866'N	004°01.579'W
9	57° 41.702' N	04° 01.565' W	57°41.867'N	004°01.514'W
10	57° 41.700' N	04° 01.520' W	57°41.706'N	004°01.506'W

## Rock Armour

6. The requirement for inclusion of rock armour at both ends of the quay are for the following purposes:
  - Placement of rock armour on the southern return of the quay is to reinstate the existing rock armoured side slope, which will need to be disturbed as part of the development; and
  - The rock revetment on the north return is to be formed to provide passive resistance and stability of the infill of the working platform. This will be constructed from a stone core and protected from wave and tidal erosion with the recovered rock armour on the outer faces.
7. As per the existing marine licence, the following materials are proposed to be removed from the site.
  - 10,868 tonnes sand
  - 30,715 tonnes crushed rock and armour stone
8. It is planned that some of the crushed rock and stone will be reused for the construction of rock armour at both ends of the quay as detailed in the accompanying drawings EIDQ-RWC-00-XX-DR-C-00004, EIDQ-RWC-00-XX-DR-C-00040, EIDQ-RWC-00-XX-DR-C-00045.
9. The quantity of material required for the proposed rock armour at both ends of the quay is as follows:

<b>Northern Revetment</b>	
Rock Armour	4,000t
Core Material	15,000t
<b>Southern Revetment</b>	
Rock Armour	1,500t
Core Material	2,500t

## Impact on Airborne Noise

### **Construction Phase**

10. Construction noise was scoped out by both the MD-LOT and The Highland Council during the EIA Scoping stage through scoping opinion issued by The Highland Council on 14<sup>th</sup> May 2024 and by MD-LOT on 4<sup>th</sup> June 2024.

11. The primary sources of noise during the construction phase considered were from the construction activities, specifically from piling, reverse alarms and plant activities affecting the residents in Balnabraich to the north, Balnapaling to the east, and Cromarty to the south and marine mammals.
12. The adjustment of the site boundary will have no effect on site activities as they will remain the same as previously detailed within the EIAR, version 1, submitted in July 2024.
13. With regards to noise generation associated with the placement of rock armour, the activity is comparable to the temporary bund construction assessed in the original assessment, but at a smaller scale. The placement of rock armour will be undertaken using a long-reach excavator, over a short duration. Therefore, the activity is not expected to generate any significant noise. In addition, it is proposed to undertake the activity during the final stage of the development, thereby avoiding any cumulative effects.
14. A Construction Noise Management Plan (CNMP) specific to the proposed activities will be submitted before the start of the proposed works. This CNMP will take into consideration the proposed change in the site boundary and will include the activities associated with the proposed rock armour placement.

### ***Operational Phase***

15. For the operational phase, the original assessment focused on the general port activities, including ship berthing and mooring, ship loading/unloading activities, including operation of cranes, movement of materials between ships and laydown/quay area, plant and HGV movements within quay and laydown/quay area, loading/unloading of HGVs; and maintenance activities.
16. As the proposed alteration to the site boundary is a minor adjustment to ensure that the works are undertaken within the licensed area, this element has no bearing on noise generation, given that the activities remain as per the original submission.
17. With regards to the noise generation from the placement of rock armour, the activity is limited to the construction phase only and no noise generation is anticipated during the operational phase.
18. Based on the above information, it can be stated that there will be no material change with respect to airborne noise as a result of the proposed variations.

## **Impact on Biodiversity**

### ***Construction Phase***

19. Given that the stones for the rock armour will be sourced from the site itself, the possibility of the introduction of Marine Invasive Non - Native Species (mINNS) is negligible, and there will be no change in the vessel movements considered in the original assessment. As the stones for the rock armour will be placed using a long-reach excavator, the possibility of a pollution event is negligible. The Construction Environmental Management (CEM) Plan includes surface water management and pollution prevention measures (e.g. Pollution Prevention Plan) that will be in place during construction and operation.
20. Sedimentation from rock armour installation will be negligible due to the small volume of coarse material and the sheltered inner dock environment. Significant distance from designated sites further ensures that any suspended sediment will dissipate with negligible effects.

### ***Operational Phase***

21. As no activity associated with rock armour is anticipated during the operational phase, and with a minor alteration of the site boundary or temporary works, no impact is anticipated to have any adverse effects on the terrestrial, coastal or marine biodiversity.
22. Based on the above information, it can be stated that there will not be any material change with respect to impacts on biodiversity due to the proposed variations

## **Impact on Traffic Assessment**

### ***Construction Phase***

23. The proposed alteration in the site boundary will have no direct impact on construction traffic; however, the proposed additional rock armour placement will add some internal Port-based HGV movements to the originally anticipated construction traffic.
24. As all the required stone and rocks will be sourced from the site itself, there will not be any additional traffic movements compared to what was considered in the original application. Therefore, the original assessment conclusion of negligible impact from the construction traffic remains unchanged. Any effects would be temporary and would continue to be managed through the implementation of a Construction Traffic Management Plan (CTMP).

### ***Operational Phase***

25. Within the original assessment, the operational phase was scoped out as only 14 additional employees were anticipated to contribute to the existing traffic during the operational phase.
26. With the proposed variation in the site boundary and rock armour placement, it is not envisaged to have any additional traffic movements during the operational phase of the development.
27. Based on the above information, it can be stated that there will not be any material change with respect to impacts on traffic assessment due to the proposed variations.

## **Impact on Water, Environmental, and Coastal Processes**

### ***Construction Phase***

28. Considering the small increase in site area and permeable nature of rock armour, the potential environmental effects in terms of surface runoff will be negligible.
29. Given the coarse nature of the rock armour material, it is considered that the potential for dispersion and discharge of sediment will be negligible.
30. Potential water quality impacts from rock armour placement will be negligible. The coarse material and confined inner-dock setting ensure that any suspended solids are localised, brief, and unlikely to affect aquatic life.
31. As the rock armour will be placed using a long-reach excavator, the possibility of a pollution event is negligible.
32. Considering the sheltered location of the proposed rock armour within the existing dry dock, the existing hard-engineered character of the shoreline, and the placement of stones of rock armour using a long-reach excavator, it is not anticipated to affect the tidal regime or wave climate.

33. With regards to sediment transport, the absence of significant impact to the key drivers, tidal currents and wave climate, and no significant change to coastal alignment, the impact will be negligible.

***Operational Phase***

34. As the proposed area of rock armour will be below the MHWS level, there will not be any impact in terms of surface runoff.
35. Given there will be no activity associated with rock armour during the operational phase, there is no potential for dispersion and discharge of sediments, as well as no impact in terms of tidal regime, wave climate and sediment transport.
36. Based on the above information, it can be stated that there will not be any material change with respect to impacts on Water, Environmental, and Coastal Processes due to the proposed variations.

**Impact on Supporting Assessments**

37. Considering the scale of alteration to the site boundary and rock armour to be placed, it is not anticipated to result in any material change in the original assessment carried out for other scoped-out supporting assessments.

## Environmental Impact Assessment (EIA) Screening Request

### Introduction

38. Global Energy Nigg Limited seek a formal Screening Opinion from Marine Scotland as the Consenting Authority under Part 2, Regulation 10(1) of the Marine EIA Regulations in respect of the proposed variation of the Marine Construction Licence MS-00010859 for Engineering Works to Form New Eastern Inner Dock Quay at Port of Nigg.
39. The approximate National Grid Reference (NGR) for the site is NH 7940 6921, with the site boundary shown in Drawing No. 683240GIS004, Appendix A.
40. As required by Part 2, Paragraph 10 of the above Regulations, we have provided information relevant to this proposal, which includes:
41. (2) A request for a screening opinion under paragraph (1) must include –
  - a) a description of the location of the proposed works, including a plan sufficient to identify the area in which the works are proposed to be sited;
  - b) a description of the proposed works, including in particular—
    - (i) a list of all of the regulated activities which are proposed;
    - (ii) a description of the physical characteristics of the proposed works and, where relevant, works to be decommissioned; and
    - (iii) a description of the location of the proposed works, with particular regard to the environmental sensitivity of geographical areas likely to be affected;
  - c) a description of the aspects of the environment likely to be significantly affected by the proposed works; and
  - d) a description of any likely significant effects, to the extent of the information available on such effects, of the proposed works on the environment resulting from either, or both, of the following:—
    - (i) the expected residues and emissions and the production of waste, where relevant;
    - (ii) the use of natural resources, in particular soil, land, water and biodiversity.
42. (3) A request for a screening opinion may, in addition to the information required in accordance with paragraph (2), also be accompanied by a description of any features of the proposed works or proposed measures envisaged to avoid or prevent significant adverse effects on the environment.
43. (4) The information referred to in paragraph (2) is to be compiled taking into account, where relevant—
  - a) the selection criteria set out in schedule 3; and
  - b) the available results of any relevant assessment.
44. The information provided is drawn from a desk-based appraisal of the site in relation to its environmental setting.

## The Site

45. The site is part of the wider Port of Nigg (PON), which comprises laydown and storage areas, fabrication and assembly shops, staff offices and a deep-water quay. The site comprises the eastern side of the Inner Dock at the PON (Ordnance Survey Grid Reference NH 7940 6921). The Inner Dock was originally constructed as a dry dock for the fabrication of steel oil production platforms for the North Sea. Currently, however, the Inner Dock is flooded, and the dock gate is moored offshore at the adjacent Oil Terminal jetty. This situation is envisaged to remain for the foreseeable future, and the proposed construction works would take place within a flooded Inner Dock.
46. PON was established in 1972 as Nigg Fabrication Yard and consists of approximately 70 hectares (ha) of land reclaimed from the eastern edge of Nigg Bay.
47. Access to the Park can be gained via the B9715.

## The Surrounding

48. PON is situated on the eastern edge of Nigg Bay, a RSPB reserve, and sits at the mouth of the Cromarty Firth, where it meets the Moray Firth (known as 'The Sutors'). The Nigg Oil Terminal is located to the immediate north of PON, with the B9175 and Fearn Peninsula to the east. The B9175 forms a part of the National Cycle Network.
49. A High Voltage (HV) Sumitomo Electrics cable manufacturing factory and ancillary facilities are to the east of the B9175 opposite PON. It is intended that the upgraded east side of the Inner Dock at PON will be used to receive the HV cables from the Sumitomo Electrics cable factory, directly onto installation vessels, predominantly to service the offshore wind market.
50. Nearby settlements include the hamlets of Balnabruich and Balnapaling to the immediate north, with Castlecraig approximately 1.5 km east, Nigg approximately 2 km north and in the wider area, Arabella Ankerville, Ballintore and the A9 further north. The village of Cromarty is located ~1.5 km south across the Cromarty Firth from PON. The Cromarty Ferry crosses the entrance to the firth to the west of The Sutors in the summer season from May to September.

## Local Sensitivities

51. The surrounding area contains several designations. These include the following
  1. Environmental Moray Firth SAC (bottlenose dolphins and sub-tidal sandbanks);
  2. Dornoch Firth and Morrich More SAC (Harbour seal);
  3. Cromarty Firth Special Protection Area (SPA) (Birds comprising Bar-tailed Godwit, Common Tern and Waterfowl assemblage);
  4. Moray Firth SPA (birds comprising Red-throated Diver, Slavonian Grebe, Greater Scaup, Common Eider, Long-tailed Duck, Common Scoter, Velvet Scoter, Common Goldeneye, Red-breasted Merganser); and
  5. Cromarty Firth Site of Special Scientific Interest (SSSI) (mudflats, sandflats and saltmarsh habitats and non-breeding waterfowl: Bar-tailed Godwit, Red-breasted Merganser, Redshank, Whooper Swan and Wigeon).

## EIA Screening

52. Under the Marine EIA Regulations, proposals are screened to determine whether they fall within one of the types or scales of development which would require an EIA. In screening an application, consideration is given to whether the proposal would fall into any of the categories set out in Schedules 1 or 2 of the Regulations.
53. The EIA Regulations do not attempt to define 'significant effects' as each development must be dealt with on its own merits. Schedule 1 of the Regulations lists types and scales of development for which an EIA will always be required. Schedule 2 of the Regulations lists types and scales of development for which an EIA might be required, subject to assessment under Schedule 3.

### **Assessment in Relation to Schedule 1**

54. The development was originally identified as being a Schedule 1 development as it fell under:

*Paragraph 8 (2) - Trading ports, piers for loading and unloading connected to land and outside ports (excluding ferry piers) which can take vessels of over 1,350 tonnes.*

### **Assessment in Relation to Schedule 2**

55. The table within Schedule 2, Paragraph 2, in which the proposed variation to the marine works could fall under is:

*Paragraph 14 - Any change to or extension of works of a description mentioned in Schedule 1 (other than a change or extension falling within paragraph 21 of Schedule 1) where those works are already authorised, executed or in the process of being executed.*

56. As such, it is considered necessary to assess the proposals against the criteria contained in Schedule 3 to establish whether or not an EIA will be necessary.

### **Assessment in Relation to Schedule 3**

57. Schedule 3 of the Regulations provides selection criteria for the screening of Schedule 2 developments. It must be noted that there are no rigid thresholds providing a universal test of whether or not an EIA is required. The proposal must be considered on a case-by-case basis by virtue of factors such as its nature, size or location. The fundamental test to be applied in each case is whether that particular type of development proposed and its specific impacts are likely, in that particular location, to result in significant effects on the environment.
58. The current design requires the following work to be conducted within the marine environment (i.e. below MHWS):
1. Enabling works – Site surveys, site clearance, office relocation, installation of temporary office and welfare facilities, delineation of traffic and pedestrian routes, etc.
  2. Demolition – Removal of revetment material /structures forming the dock side slopes and quay furniture.
  3. Temporary structures – construction of a temporary piling platform.
  4. Quay Wall Structure - front quay wall, rear anchor wall and tie rods.
  5. Earthworks – Placement of imported fill material behind the new quay wall.
  6. Reinforced Concrete Works – Cope beam.
  7. Drainage – Positive Surface Water Drainage System.
  8. Electrical Works – High Mast Lighting System.
  9. Concrete Slab – levelling and construction of concrete surface slab.
  10. Rock Armour – placement of rock armour at both ends of the quay.

59. The main concerns in relation to these marine construction works identified in the 2024 EIAR were:
1. Potential airborne noise impacts from construction activities, specifically from piling, reverse alarms and plant activities;
  2. Potential airborne noise impacts from HV cable loading events and general site activities;
  3. Potential suspended sediments within the water due to the removal of rock armour and the construction of the temporary piling platform;
  4. Potential impacts of surface water flow alterations and increased run-off to coastal waters;
  5. Potential risk of a pollution incident due to construction and operational activities;
  6. Potential alterations to the local tidal regime and local wave climate;
  7. Potential sedimentation transportation;
  8. Potential visual and noise disturbance to fauna due to construction activities;
  9. Potential risk of collision and disturbance of marine mammals as well as introduction and spread of marine Invasive Non-Native Species (INNS) due to marine traffic; and
  10. Impact on local road network;
60. The 2024 EIAR considered the above and defined appropriate mitigation measures to limit any impact as summarised in the Schedule of Mitigation and resulting Construction Environmental Management Document (CEMD). We are therefore of the opinion that the proposed variation to the marine construction licence will not have a significant impact on the outcomes of the 2024 EIAR assessments.

## Conclusions

In conclusion, we are of the opinion that the proposed variations to the Marine Construction Licence will have no significant impacts on the site or the surrounding area and its notable environmental features.

## **A DRAWINGS**

279200

279300

279400

279500

869300

869200

869100



**Legend**

- New Site Boundary
- New boundary points

Do not scale this map

**Client**  
Global Energy Nigg Ltd

**Project**  
EIA Variation Application

**Title**  
New Site Boundary

**Status**  
FINAL

<b>Drawing No.</b> 683240-GIS004	<b>Revision</b> -	<b>Date</b> 18 Feb 2026
<b>Drawn</b> JSH	<b>Checked</b> CCAS	<b>Approved</b> CCAS

**Scale**  
1:1,300 @ A3<sup>0</sup> 25 50 m

Rev	Date	Amendment	Initials
-	-	-	-



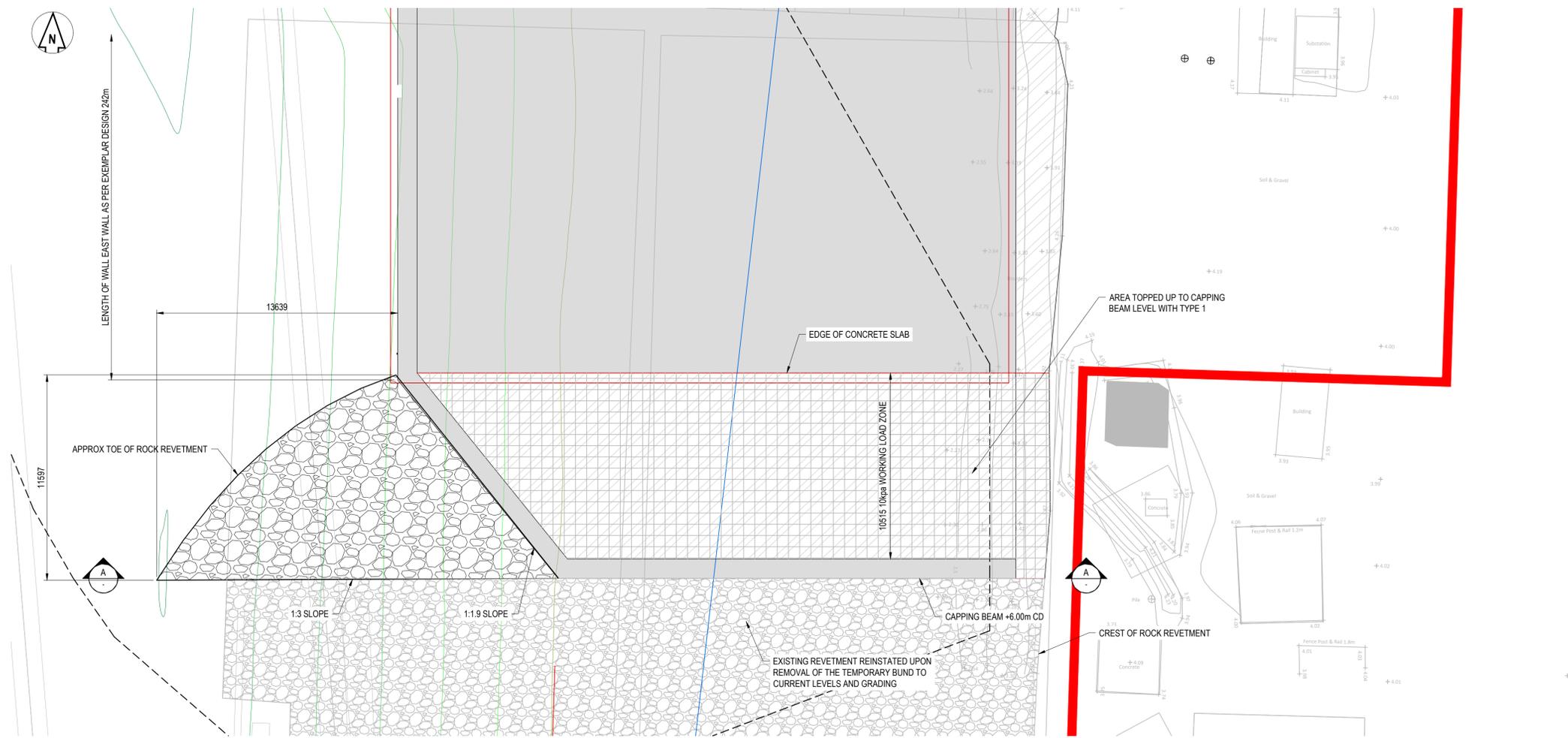
part of CuraTerra

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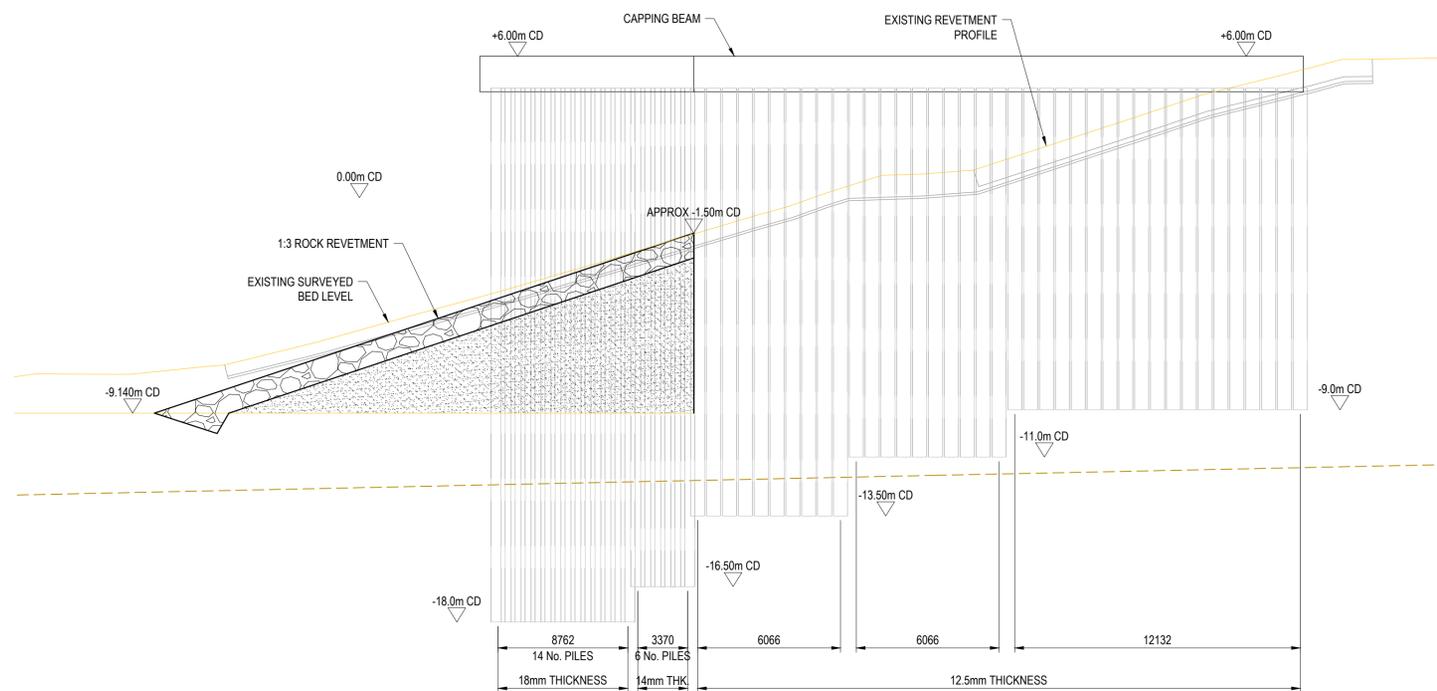
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**NOTES**

1. The contractor is responsible for verifying all site & setting out dimensions before commencing work.
2. This drawing is to be read in conjunction with all relevant Architectural and M&E drawings.
3. All dimensions in millimetres unless stated otherwise.
4. For general notes refer to drawing ref. EIDQ-RWC-00-XX-DR-C-00001.



**PLAN**  
SCALE 1:150



**SECTION A-A**  
SCALE 1:150

LEGEND	
	Indicates a residual risk warning

Revision History					
Rev	Comment	By	Chkd	Appr	Date
P02	REVIEMENT & PILES AMENDED	GW	RF	RK	23/12/2025
P01	FIRST ISSUE	GW	RF	RK	08/12/2025
Current Revision					
P03	MINOR AMENDMENTS	GW	RF	RK	19/01/2026

**FOR COMMENT**

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Project  
PORT OF NIGG  
EASTERN INNER DOCK QUAY

Drawing Title  
SOUTHERN RETURN WALL  
ROCK REVIEMENT

RWCL Internal Register reference: 5783-006

Scales @ A1

Scale 1:150 @ A1 - 1:300 @ A3

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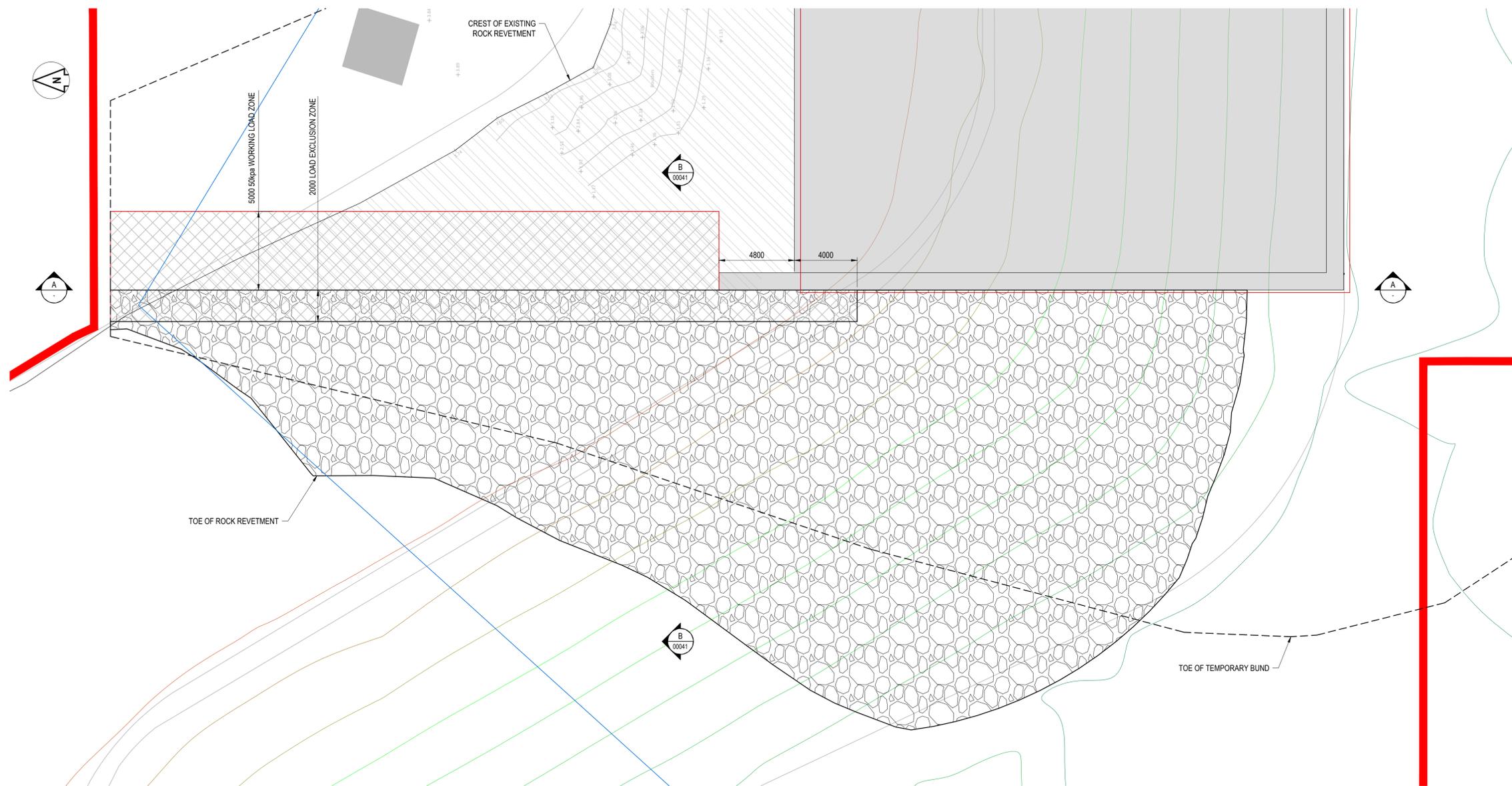
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EIDQ-RWC-00-XX-DR-C-00045 - P03

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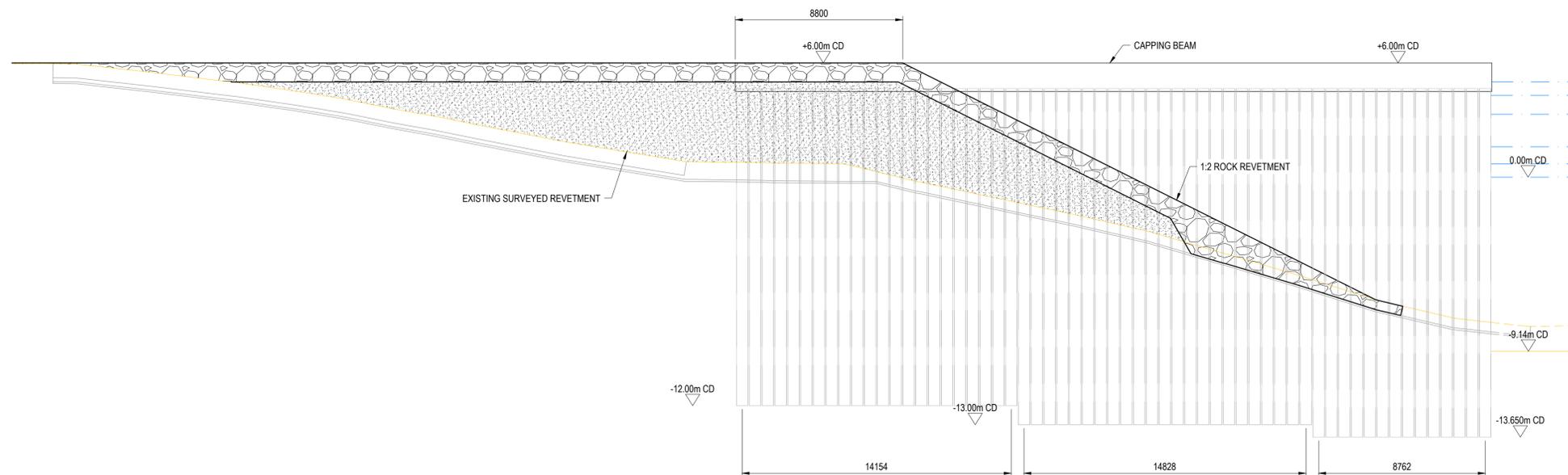
**NOTES**

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**PLAN**

SCALE 1:150



**A SECTION A-A**

SCALE 1:150

LEGEND	
	Indicates a residual risk warning

Revision History					
Rev	Comment	By	Chkd	Appr	Date
P02	PILE SHOE REMOVED	GW	RF	RK	23/12/2025
P01	FIRST ISSUE	GW	RF	RK	08/12/2025
Current Revision					
P03	REVETMENT TOE AMENDED	GW	RF	RK	19/01/2026

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**NIGG**

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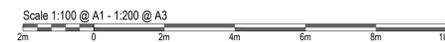
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Project  
PORT OF NIGG  
EASTERN INNER DOCK QUAY

Drawing Title  
NORTHERN RETURN WALL  
ROCK REVETMENT

RWCL Internal Register reference: 5783-006 Scales @ A1

EIDQ-RWC-00-XX-DR-C-00040 -P03

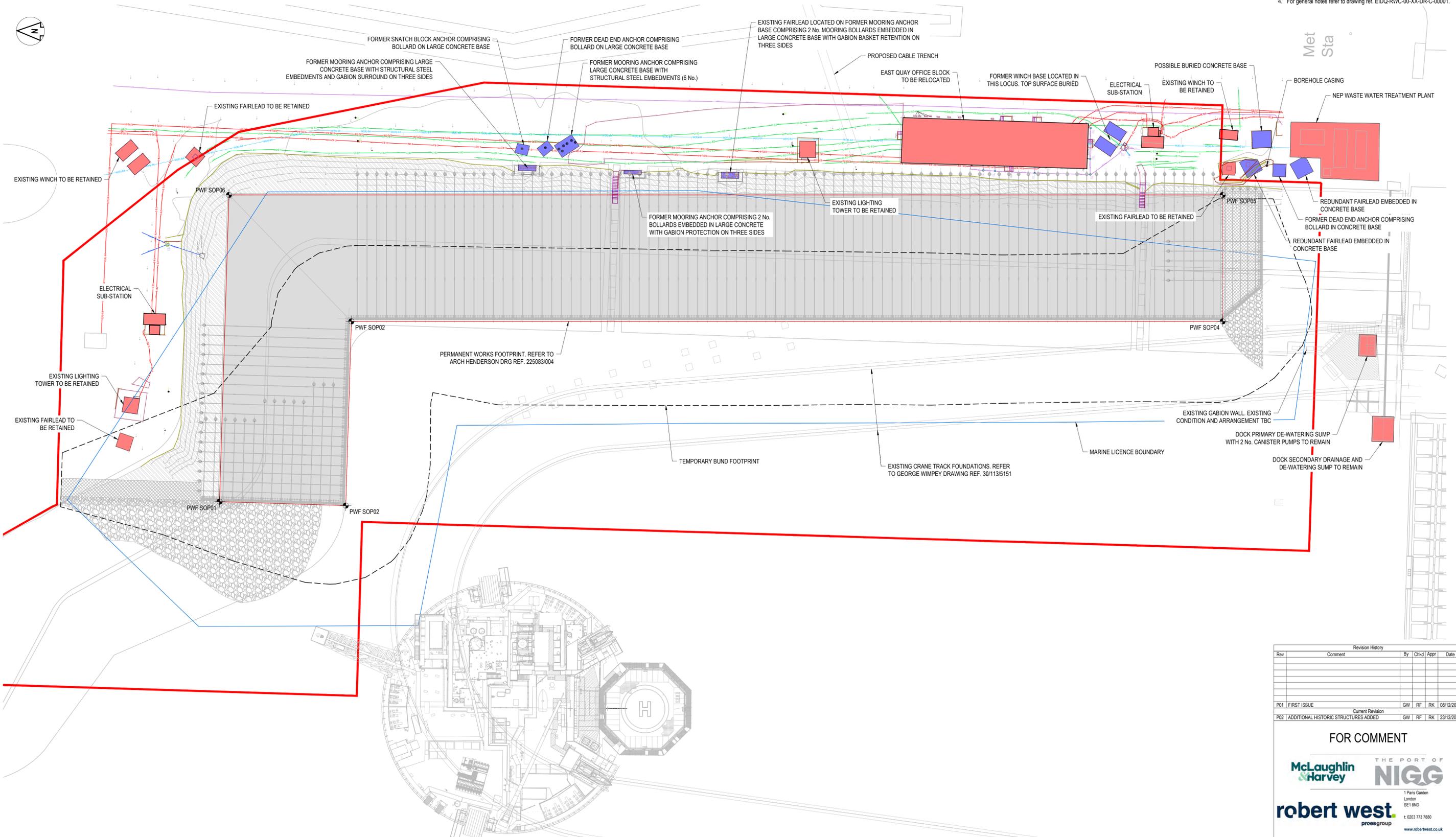


- KEY:
- STRUCTURES TO BE RETAINED
  - STRUCTURES TO BE REMOVED

LEGEND

Indicates a residual risk warning

- DO NOT SCALE OFF THIS DRAWING
- NOTES
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  3. All dimensions in millimetres unless stated otherwise.
  4. For general notes refer to drawing ref. EIDQ-RWC-00-XX-DR-C-00001.



**PERMANENT WORKS FOOTPRINT SETTING OUT**

SOP	EASTINGS (m)	NORTHINGS (m)
01	279297.950	869348.177
02	279297.950	869313.177
03	279348.959	869313.177
04	279356.430	869071.755
05	279391.413	869072.837
06	279382.893	869348.177

Revision History

Rev	Comment	By	Chkd	Appr	Date
P01	FIRST ISSUE	GW	RF	RK	08/12/2025
P02	ADDITIONAL HISTORIC STRUCTURES ADDED	GW	RF	RK	23/12/2025

FOR COMMENT

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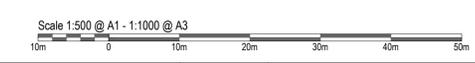
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Project  
PORT OF NIGG  
EASTERN INNER DOCK QUAY

Drawing Title  
GENERAL ARRANGEMENT  
EXISTING STRUCTURES

RWCL Internal Register reference: 5783-006 Scales @ A1

EIDQ-RWC-00-XX-DR-C-00004 - P02



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