

Briggs Marine Contractors

Proposed Installation of Burntisland Pontoon

Prepared by

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Registered Office: Seaforth House, Seaforth Place, Burntisland, Fife, KY3 9AX • Registered in Scotland • Registration No. 114978 • VAT No. GB 502 9444 61

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Introduction & Scope

The below has been prepared in order to outline the placement of a pontoon in East dock Burntisland to accommodate CTV's in the growing offshore wind sector.

The pontoon, once successfully in place will provide a safe embarkation and disembarkation point for crew, technicians, company representatives and various other staff required to transit to the windfarm via CTV.

Pontoon Details

| | |
|-------------------------------|---------------------|
| LOA | 16.2m |
| Breadth | 3.2m |
| Breadth (Including Fendering) | 3.4m |
| Depth | 1.24m |
| Draft | Approximately 400mm |
| Year of Build | 2010 |
| Weight | Approximately 22t |

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Deployment Details

Proposed Placement

Our proposed position of placement will be on the western wall to the north of the gate. This area is secure and will mean operations will not have any inadvertent effects on other parties using the dock such as Calypso Marine or the local sailing club.

This area also has a suitable depth to accommodate both the pontoon and CTV and is close to our welfare unit which we already have in place next to unit 1.

A – 56°03.348'N 003°14.114'W

B – 56°03.347'N 003°14.118'W

C – 56°03.338'N 003°14.110'W

D – 56°03.339'N 003°14.107'W

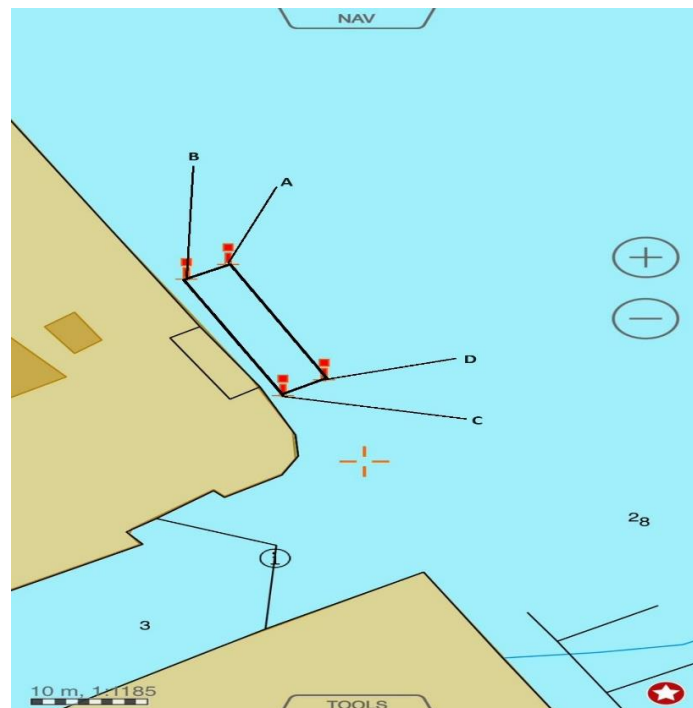


Figure 1 - Charted Position

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Deployment

Our intention is to deploy the pontoon in west dock via a basket lift under the hull utilising our heavy lift crane.

Once waterborne the pontoon will be towed to position in east dock where it will be connected to its moorings.

A full lift plan will be completed prior to commencement.



Figure 2 – Pontoon Lifting Arrangement (KOF pictured carrying out the lift however the slinging arrangement will remain the same)

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Mooring Arrangement

Once in place the pontoon will be secure to the wall using 38mm U2 Grade steel OEL Chain run through 2 separate splittable spurling pipes on the aft section of the hull in between the rear fenders.

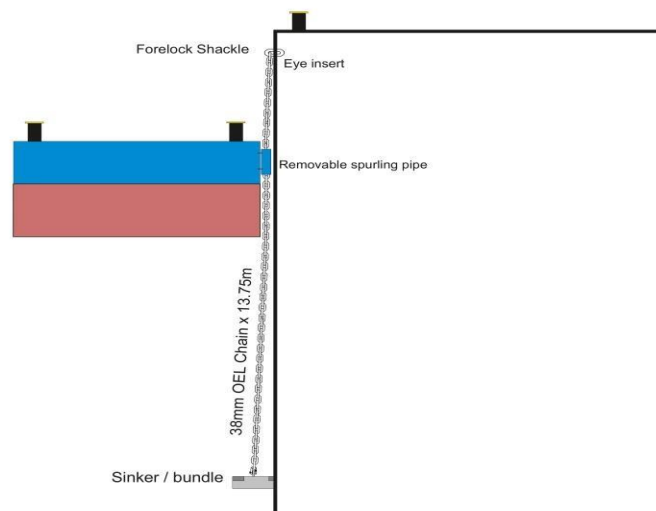
The chain will be connected to the wall via two eyes just below the quay edge held in place with a resin concrete anchor. The chain itself will be left longer than the depth at 13.75m in each length meaning some residual chain will remain on the bed to allow for weight in the system. The chain will finally be connected to a sinker to hold it in place.

Forces exerted on the pontoon will be primarily towards the wall keeping the majority of any stress on the rear fenders and off the mooring system however in any case external forces acting the opposite way such as windage, tides etc will have no effect on the system due to the excessive weight in the chain.

As the spurling pipes will be designed to open we will have the ability to easily disconnect the pontoon should it be required for maintenance on the pontoon itself or the mooring system.



Burntisland Pontoon Proposed Mooring Arrangement



This arrangement will allow the pontoon to rise and fall on the tide but maintain position.

Fig – 3
Mooring Arrangement

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Pontoon Maintenance

The pontoon is currently shoreside in west dock undergoing a full refurbishment prior to deployment.

This includes.

- Replacement plating around various sections of the hull.
- New fendering.
- Installation of new anodes.
- New antifouling and paint.
- Anti-slip on the deck itself.
- Repairs to the handrails surrounding the deck.



Fig-4 Thickness testing & Painting.

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Fig-5 Old fendering to be replaced with new.

Gangway Arrangement

Once secured on the moorings access will be established to the pontoon via a mild steel cantilever gangway connected to a turntable on the quayside for horizontal movement of up to 15°. The gangway itself will accommodate a vertical moment up to 30° which it will offset by lateral movement at the base up and down the pontoon.



Fig-6 Example Gangway arrangement.

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Conditions of use

Once in place and inspected the pontoon will operate as our primary embarking and disembarking point for the CTV which will berth port side alongside for her boarding point just aft of the accommodation.

When not in use with the CTV, Briggs are more than happy to allow Forth Ports use of the pontoon for Pilot vessels etc however the CTV must take priority in all instances. Any damage incurred to the system when in use by Forth Port vessels will be liable to Forth Ports.

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